

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS

ADDENDUM NO. 2

TO

SPECIAL PROVISIONS, SPECIFICATIONS, PROPOSAL

FOR

RUNWAY 17-35 REHABILITATION

AT

ELLISON ONIZUKA KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

STATE PROJECT NO. AH2021-16

December 4, 2023

This Addendum shall make the following amendments to the Bid Documents.

A. NOTICE TO BIDDERS

1. Delete NOTICE TO BIDDERS dated October 26, 2023 in its entirety and replace with the attached NOTICE TO BIDDERS dated r12/04/2023.

Bidders are advised that the deadline to submit bids is hereby POSTPONED until December 18, 2023, at 2:00 p.m., Hawaii Standard Time.

B. SPECIAL PROVISIONS

1. Delete SPECIAL PROVISIONS dated September 2023 in its entirety and replace with the attached SPECIAL PROVISIONS dated r12/04/2023.

C. SPECIFICATIONS

1. Delete TABLE OF CONTENTS page TOC – 3 dated September 2023, and replace with the attached TABLE OF CONTENTS page TOC - 3 dated r12/04/2023.
2. Add SECTION 01351-ESCROW PROPOSAL DOCUMENTATION (EPD) dated 12/04/2023.

3. Delete SECTION 16101-AIRFIELD ELECTRICAL DEMOLITION AND REMOVAL dated September 2023 in its entirety and replace with the attached SECTION 16101-AIRFIELD ELECTRICAL DEMOLITION AND REMOVAL dated r12/04/2023.

D. PLANS

1. Delete Dwg G-101 and replace with Addendum No. 2 Dwg G-101 dated r12/04/23.
2. Delete Dwg G-123 and replace with Addendum No. 2 Dwg G-123 dated r12/04/23.
3. Delete Dwg C-301 and replace with Addendum No. 2 Dwg C-301 dated r12/04/23.
4. Delete Dwg C-302 and replace with Addendum No. 2 Dwg C-302 dated r12/04/23.
5. Delete Dwg C-303 and replace with Addendum No. 2 Dwg C-303 dated r12/04/23.
6. Delete Dwg C-304 and replace with Addendum No. 2 Dwg C-304 dated r12/04/23.
7. Delete Dwg C-305 and replace with Addendum No. 2 Dwg C-305 dated r12/04/23.
8. Delete Dwg C-306 and replace with Addendum No. 2 Dwg C-306 dated r12/04/23.
9. Delete Dwg C-307 and replace with Addendum No. 2 Dwg C-307 dated r12/04/23.
10. Delete Dwg C-308 and replace with Addendum No. 2 Dwg C-308 dated r12/04/23.
11. Delete Dwg C-309 and replace with Addendum No. 2 Dwg C-309 dated r12/04/23.

The following is provided for information:

E. PRE-BID MEETING MINUTES

1. The pre-bid meeting minutes dated November 6, 2023, are included in this addendum for information purposes only.

F. RESPONSE TO REQUEST FOR INFORMATION (RFI's) / QUESTIONS

1. Responses to RFI's submitted in HIePRO are provided in the attached document dated 11/27/2023, for information only.

G. ATTACHMENTS

1. The following files shall be made part of Addendum No. 2. These files are provided for information only. The Contractor shall rely on the Contract Documents when preparing their bids:
 - a. Modified Phase 1 Environmental Site Assessment
https://download.veoci.com/fhN2DPYIHDErPDDQ/120037-15_KOA%20Phase%20I%20Report%20Final_052721_.pdf

- b. Contaminated Soil and Groundwater Review dated 2/3/2023.
- c. Geotechnical Data Report, Runway 17-35 Rehabilitation at Ellison Onizuka Kona International Airport dated December 2021.
- d. AH2021-16-DESIGN-FG.dwg
- e. AH2021-16-EXST-SURF.dwg

Please acknowledge receipt of this Addendum No. 2 by recording the date of its receipt in the space provided on Page P-4 of the Proposal Schedule.



EDWIN H. SNIFFEN
Director of Transportation

NOTICE TO BIDDERS
Hawaii Revised Statutes (HRS),
Chapter 103D

SEALED BIDS for RUNWAY 17-35 REHABILITATION, ELLISON ONIZUKA KONA INTERNATIONAL AIRPORT AT KEAHOLE, KAILUA-KONA, HAWAII,

PROJECT NO. AH2021-16, AIP PROJECT NO. 3-15-0008-##, will begin as advertised on HiePRO. Bidders are to register and submit bids through HiePRO only. See the following HiePRO link for important information on registering: <https://hiepro.ehawaii.gov/welcome.html>.

Plans, specifications, proposal, contract forms, and any other applicable documents may be obtained from HiePRO.

DEADLINE TO SUBMIT BIDS is December 18, 2023, at 2:00 p.m., Hawaii Standard Time (HST). **Bidders shall submit and upload the complete proposal to HiePRO prior to the bid opening date and time. Proposals received after said due date and time shall not be considered. Any additional support documents explicitly designated as confidential and/or proprietary shall be uploaded as a separate file to HiePRO. Do not include confidential and/or proprietary documents with the proposal. The record of each bidder and respective bid shall be open to public inspection. FAILURE TO UPLOAD THE PROPOSAL TO HiePRO SHALL BE GROUND FOR REJECTION OF THE BID.**

The scope of work consists of rehabilitation of Runway 17-35 and connecting taxiways including lighting and signage changes. The rehabilitation will consist of asphalt pavement mill and overlay, new asphalt pavement, and new concrete pavement. The estimated cost of construction is between \$95,000,000.00 and \$105,000,000.00.

To be eligible for award, bidders must possess a valid State of Hawaii General Engineering "A" license prior to the award of contract.

The GENERAL PROVISIONS dated 2016, applicable to this project are available at <http://hidot.hawaii.gov/administration/con/>.

A pre-bid conference is scheduled for 2:00 P.M. HST on Monday, November 6, 2023, on Microsoft Teams. All prospective bidders or their representatives (employees) are encouraged to attend, but attendance is not mandatory. All bidders that wish to attend must send an email indicating their interest to Mr. Eddie Chiu, State Project Manager, at eddie.k.chiu@hawaii.gov. They will be added to the Microsoft Teams attendance list and will be sent an invitation email with a Microsoft Teams web-link. This will allow each person to attend the pre-bid conference via the internet. The invitation will also contain teleconference information, so bidders may call in instead. The deadline to sign up for the pre-bid conference is one (1) working day prior to the date of the pre-bid conference. All information presented at the pre-bid conference is for clarification and information only, as any amendments to the bid documents shall be made by formal addendum and posted in HIePRO.

All Request for Information (RFI) questions and substitution requests shall be submitted via HIePRO no later than November 27, 2023, at 2:00 p.m., HST. Questions received after the deadline will not be addressed. Verbal RFIs will not receive a response.

Campaign contributions by State and County Contractors. Contractors are hereby notified of the applicability of HRS §11-355 which states that campaign contributions are prohibited from specified State or county government contractors during the term of the contract if the contractors are paid with funds appropriated by a legislative body. For more information, contact the Campaign Spending Commission at (808) 586-0285.

Protests. Any protest of this solicitation shall be submitted in writing to the Director of Transportation, in accordance with HRS §103D-701 and Hawaii Administrative Rules §3-126.

The Equal Employment Opportunity Regulations of the Secretary of Labor implementing Executive Order 11246, as amended shall be complied with on this project.

The U.S. Department of Transportation Regulation entitled, “Nondiscrimination in Federally Assisted Programs of the U.S. Department of Transportation,” Title 49, Code of Federal Regulations (CFR), Part 21 is applicable to this project. Bidders are hereby notified that the Department of Transportation will affirmatively ensure that the contract entered into pursuant to this advertisement will be awarded to the lowest responsible bidder without discrimination on the grounds of race, color, national origin or sex (as directed by 23 CFR Part 200).

The U.S. Department of Transportation Regulations entitled "Participation by Disadvantaged Business Enterprise in Department of Transportation Financial Assistance Programs", Title 49, CFR, Part 26 is applicable to this project. Bidders are hereby notified that the Department of Transportation will strictly enforce full compliance with all of the requirements of the Disadvantaged Business Enterprise program with respect to this project.

Bidders are directed to read and be familiar with the DBE Requirements, which establishes the program requirements pursuant to Title 49 Code of Federal Regulations Part 26 and, particularly, the requirements of certification, method of award, and evidence of good faith. All Bidders must e-mail the State Project Manager at eddie.k.chiu@hawaii.gov, the Disadvantaged Business Enterprise Contract Goal Verification and Good Faith Efforts Documentation for Construction, the Disadvantaged Business Enterprise Confirmation and Commitment Agreement – Trucking Company, and the Disadvantaged Business Enterprise Confirmation and Commitment Agreement – Subcontractor, Manufacturer, or Supplier by the close of business, 4:30 p.m., HST, on December 26, 2023. Failure to provide these documents shall result in rejection of bid.

For additional information, contact Mr. Eddie Chiu, Project Manager, by phone at (808)838-8827 or email at eddie.k.chiu@hawaii.gov.

The State reserves the right to reject any or all proposals and to waive any defects in said proposals for the best interest of the public.



EDWIN H. SNIFFEN
Director of Transportation

Posted on HiePRO:

SPECIAL PROVISIONS

The following additional amendments to the General Provisions are applicable to this project:

1.3 DEFINITIONS is amended as follows:

The definition for Subcontractor is deleted in its entirety and replaced with the following:

Subcontractor – An individual, partnership, firm, corporation, joint venture or other legal entity, as licensed or required to be licensed under Chapter 444, Hawaii Revised Statutes, as amended, which enters into an agreement with the Contractor to perform a portion of the work.

The following definitions shall be added:

AASHTO - The American Association of State Highway and Transportation Officials.

Access Road - The right-of-way, the roadway and all improvements constructed thereon connecting the airport to a public roadway.

Airport Improvement Program (AIP) - A grant-in-aid program, administered by the Federal Aviation Administration (FAA).

Air Operations Area (AOA) - The term air operations area (AOA) shall mean any area of the airport used or intended to be used for the landing, takeoff, or surface maneuvering of aircraft. An air operation area shall include such paved or unpaved areas that are used or intended to be used for the unobstructed movement of aircraft in addition to its associated runway, taxiway, or apron.

Apron - Area where aircraft are parked, unloaded or loaded, fueled and/or serviced.

ASTM International (ASTM) - Formerly known as the American Society for Testing and Materials (ASTM).

Building Area - An area on the airport to be used, considered, or intended to be used for airport buildings or other airport facilities or rights-of-way together with all airport buildings and facilities located thereon.

Certificate of Analysis (COA) - The COA is the manufacturer's Certificate of Compliance (COC) including all applicable test results required by the specifications.

Certificate of Compliance (COC) - The manufacturer's certification stating that materials or assemblies furnished fully comply with the requirements of the contract. The certificate shall be signed by the manufacturer's authorized representative.

Contractors Quality Control (QC) Facilities - The Contractor's QC facilities in accordance with the Contractor Quality Control Program (CQCP).

Contractor Quality Control Program (CQCP) - Details the methods and procedures that will be taken to assure that all materials and completed construction required by the contract conform to contract plans, technical specifications and other requirements, whether manufactured by the Contractor, or procured from subcontractors or vendors.

Control Strip - A demonstration by the Contractor that the materials, equipment, and construction processes results in a product meeting the requirements of the specification.

Construction Safety and Phasing Plan (CSPP) - The overall plan for safety and phasing of a construction project developed by the airport operator, or developed by the airport operator's consultant

and approved by the airport operator. It is included in the invitation for bids and becomes part of the project specifications.

Drainage System - The system of pipes, ditches, and structures by which surface or subsurface waters are collected and conducted from the airport area.

Extra Work - An item of work not provided for in the awarded contract as previously modified by change order or supplemental agreement, but which is found by the Owner's Engineer or Resident Project Representative (RPR) to be necessary to complete the work within the intended scope of the contract as previously modified.

FAA - The Federal Aviation Administration. When used to designate a person, FAA shall mean the Administrator or their duly authorized representative.

Federal Specifications - The federal specifications and standards, commercial item descriptions, and supplements, amendments, and indices prepared and issued by the General Services Administration.

Force Account – a) Contract Force Account - A method of payment that addresses extra work performed by the Contractor on a time and material basis. b) Owner Force Account - Work performed for the project by the Owner's employees.

Hawaii eProcurement System (HiePRO) – The State of Hawaii eProcurement System for issuing solicitations, receiving proposals and responses, and issuing notices of award.

Intention of Terms - Whenever, in these specifications or on the plans, the words “directed,” “required,” “permitted,” “ordered,” “designated,” “prescribed,” or words of like import are used, it shall be understood that the direction, requirement, permission, order, designation, or prescription of the Engineer and/or Resident Project Representative (RPR) is intended; and similarly, the words “approved,” “acceptable,” “satisfactory,” or words of like import, shall mean approved by, or acceptable to, or satisfactory to the Engineer and/or RPR, subject in each case to the final determination of the Owner. Any reference to a specific requirement of a numbered paragraph of the contract specifications or a cited standard shall be interpreted to include all general requirements of the entire section, specification item, or cited standard that may be pertinent to such specific reference.

Lighting - A system of fixtures providing or controlling the light sources used on or near the airport or within the airport buildings. The field lighting includes all luminous signals, markers, floodlights, and illuminating devices used on or near the airport or to aid in the operation of aircraft landing at, taking off from, or taxiing on the airport surface.

Major and Minor Contract Items - A major contract item shall be any item that is listed in the proposal, the total cost of which is equal to or greater than 20% of the total amount of the award contract. All other items shall be considered minor contract items.

Modification of Standards (MOS) - Any deviation from standard specifications applicable to material and construction methods in accordance with FAA Order 5300.1.

Owner - The term “Owner” shall mean the party of the first part or the contracting agency signatory to the contract. Where the term “Owner” is capitalized in this document, it shall mean airport Sponsor only. The Owner for this project is the State of Hawaii, Department of Transportation, Airports.

Passenger Facility Charge (PFC) - Per 14 Code of Federal Regulations (CFR) Part 158 and 49 United States Code (USC) § 40117, a PFC is a charge imposed by a public agency on passengers enplaned at a commercial service airport it controls.

Pavement Structure - The combined surface course, base course(s), and subbase course(s), if any,

considered as a single unit.

Project - The agreed scope of work for accomplishing specific airport development with respect to a particular airport.

Quality Assurance (QA) - Owner's responsibility to assure that construction work completed complies with specifications for payment.

Quality Control - Contractor's responsibility to control material(s) and construction processes to complete construction in accordance with project specifications.

Quality Assurance (QA) Inspector - An authorized representative of the Engineer and/or Resident Project Representative (RPR) assigned to make all necessary inspections, observations, tests, and/or observation of tests of the work performed or being performed, or of the materials furnished or being furnished by the Contractor.

Quality Assurance (QA) Laboratory - The official quality assurance testing laboratories of the Owner or such other laboratories as may be designated by the Engineer or RPR. May also be referred to as Engineer's, Owner's, or QA Laboratory.

Resident Project Representative (RPR) - The individual, partnership, firm, or corporation duly authorized by the Owner to be responsible for all necessary inspections, observations, tests, and/or observations of tests of the contract work performed or being performed, or of the materials furnished or being furnished by the Contractor, and acting directly or through an authorized representative.

Runway - The area on the airport prepared for the landing and takeoff of aircraft.

Runway Safety Area (RSA) - A defined surface surrounding the runway prepared or suitable for reducing the risk of damage to aircraft. See the construction safety and phasing plan (CSPP) for limits of the RSA.

Safety Plan Compliance Document (SPCD) - Details how the Contractor will comply with the CSPP.

Sponsor - A Sponsor is defined in 49 USC § 47102(24) as a public agency that submits to the FAA for an AIP grant; or a private Owner of a public-use airport that submits to the FAA an application for an AIP grant for the airport.

Subgrade - The soil that forms the pavement foundation.

Supplemental Agreement - A written agreement between the Contractor and the Owner that establishes the basis of payment and contract time adjustment, if any, for the work affected by the supplemental agreement. A supplemental agreement is required if: (1) in scope work would increase or decrease the total amount of the awarded contract by more than 25%; (2) in scope work would increase or decrease the total of any major contract item by more than 25%; (3) work that is not within the scope of the originally awarded contract; or (4) adding or deleting of a major contract item.

Taxilane - A taxiway designed for low speed movement of aircraft between aircraft parking areas and terminal areas.

Taxiway - The portion of the air operations area of an airport that has been designated by competent airport authority for movement of aircraft to and from the airport's runways, aircraft parking areas, and terminal areas.

Taxiway/Taxilane Safety Area (TSA) - A defined surface alongside the taxiway prepared or suitable for reducing the risk of damage to an aircraft. See the construction safety and phasing plan (CSPP) for limits of the TSA.

2.6 EXAMINATION OF PLANS, SPECIFICATIONS, AND SITE OF WORK; PATENT AMBIGUITIES; REQUESTS FOR CLARIFICATION is amended as follows: The fourth paragraph (line 128 to 135) shall be replaced with the following:

“(c) A written request for clarification shall be submitted to the Department for review at the earliest date possible; but, in any event, such request must be submitted in writing in HIePRO under the question/answer tab not later than fourteen (14) calendar days before the bid opening date, not including the bid opening date.”

2.7 REQUEST FOR SUBSTITUTION OF SPECIFIED MATERIALS AND EQUIPMENT BEFORE BID OPENING is amended as follows:

1. The last sentence in the first paragraph (line 147 to 152) shall be replaced with the following:

“Where a bidder intends to use a material or equipment of an unspecified brand, make, or model, the bidder must submit a request to the Department for review and approval at the earliest date possible. Requests shall be submitted via email to the Contact person listed in HIePRO for the solicitation and also posted as a question in HIePRO under the question/answer tab referencing the email with the request. The request must be posted in HIePRO no later than seventeen (17) calendar days before the bid opening date, not including the bid opening date.”

2. The first sentence in the second paragraph (line 154 to 156) shall be replaced with the following:

“It shall be the responsibility of the bidder to submit sufficient evidence based upon which a determination can be made by the Department that the alternate brand is a qualified equivalent.”

2.8 PREPARATION AND DELIVERY OF BID is amended as follows: Last Paragraph (line 189 to 192) shall be replaced with the following:

“Bidders shall submit and upload the complete proposal to HIePRO prior to the bid opening date and time. Proposals received after said due date and time shall not be considered. Original (wet ink, hard copy) proposal documents are not required to be submitted. Contract award shall be based on evaluation of proposals submitted and uploaded to HIePRO. Any additional support documents explicitly designated as confidential and/or proprietary shall be uploaded as a separate file to HIePRO. Do not include confidential and/or proprietary documents with the proposal. The record of each bidder and respective bid shall be open to public inspection.

FAILURE TO UPLOAD THE COMPLETE PROPOSAL TO HIePRO SHALL BE GROUNDS FOR REJECTION OF THE BID.

If there is a conflict between the specification document and the HIePRO solicitation, the specifications shall govern and control, unless otherwise specified.”

2.11 BID SECURITY is amended by deleting (a) and replacing it with:

“(a) Unless directed otherwise in the invitation for bids, each bid shall be accompanied by bid security which is intended to protect the Department against the failure or refusal of a bidder to execute the contract for the work bid or to supply the required performance and payment bonds. Bid security shall be in an amount equal to at least five percent of the base bid and additive alternates. Bid security shall be in one of the following forms:

- (1) A deposit of legal tender;
- (2) A valid surety bid bond, underwritten by a company licensed to issue bonds in the State of Hawaii; or

- (3) A certificate of deposit; credit union share certificate; or cashier's, treasurer's, teller's, or official check drawn by or a certified check accepted by a bank, savings institution, or credit union insured by the Federal Deposit Insurance Corporation (FDIC) or the National Credit Union Administration (NCUA) and payable at sight or unconditionally assigned to the Department. These instruments may be utilized only to a maximum of one hundred thousand dollars (\$100,000.00). If the required amount totals over one hundred thousand dollars (\$100,000.00), more than one instrument not exceeding one hundred thousand dollars (\$100,000.00) each and issued by different financial institutions shall be accepted.

If bidder elects options (1) or (3) above for its bid security, said bid security shall be in its original form and shall be submitted before the bid deadline to the Contract Office, Department of Transportation, Aliiimoku Hale, 869 Punchbowl Street, Room 105, Honolulu, Hawaii 96813. **Original surety bid bonds do not need to be submitted to the Contracts Office. Bidders are reminded that a copy of its bid bond shall be included with its bid uploaded to HIePRO.**

2.12 PRE-OPENING MODIFICATION OR WITHDRAWAL OF BIDS is amended by deleting 2.12 PRE-OPENING MODIFICATION OR WITHDRAWAL OF BIDS in its entirety and replacing with the following:

"2.12 PRE-OPENING MODIFICATION OR WITHDRAWAL OF BIDS. A bidder may withdraw or modify a proposal after the bidder submits the proposal in HIePRO. Withdrawal or modification of proposal must be completed before the time set for the receiving of bids.

2.14 PUBLIC OPENING OF BIDS is amended by deleting 2.14 PUBLIC OPENING OF BIDS in its entirety.

4.12 UTILITIES AND SERVICES is amended as follows:

Add the following after the last paragraph:

"(e) Repairs and Outages.

- (1) The Contractor shall have available on 24-hour call sufficient specialty contractors, such as electrical and plumbing contractors, to repair any, damage to existing facilities that might occur as a result of construction operations regardless of when the damage might occur.
- (2) Outage: Written requests for power outage, communication changes, and water and sewer connection outages shall be submitted to the Engineer at least fourteen (14) calendar days in advance or as specified in other sections of these specifications. Outages will be restricted to non-peak operational hours between midnight and 6:00 a.m."

5.16 SUBCONTRACTS is amended by adding the following after Paragraph (a) (line 576):

"The State designates the following items as "specialty items"

- (1) Asphalt concrete patching, sealing, and striping."

7.1 INSURANCE REQUIREMENTS is amended by deleting Paragraph (b)(4) in its entirety and replacing with the following:

"(4) Builder's Risk For All Work. The Contractor shall take out a policy of builder's risk insurance for \$20,000,000.00, from a company licensed or otherwise authorized to do business in the State of Hawaii; naming the State as an additional insured under each policy; and covering all work, labor, and materials

furnished by such Contractor and all its subcontractors against loss by fire, windstorm, tsunamis, earthquakes, lightning, explosion, other perils covered by the standard Extended Coverage Endorsement, vandalism, and malicious mischief.”

7.4 WORKING HOURS; NIGHT WORK is amended as follows: Paragraph shall be replaced with the following:

“7.4 Working Hours. Normal working hours shall be as shown on the Plans.”

7.21 PUBLIC CONVENIENCE AND SAFETY - is hereby added to the General Provisions:

"It shall be especially noted by the Contractor that the area directly adjacent to the existing in use runways and taxiways, is an extremely hazardous area and that very strict controls will apply throughout the entire period required to complete all work within 500 feet from the edge of an in use runway and 180 feet from the edge of an in use taxiway.

The Contractor shall familiarize himself with the Airport Certification Manual available for review at the Airport Manager's Office and shall comply with its requirements.

The Contractor is responsible for the security of access points to the Airport Operational Area that are located within the limits of construction and will be fined \$1,000 per incident for any breach of security at these locations. All gates leading into the AOA shall be kept locked and if required to be open, the Contractor shall provide professional security guards to attend gates. The guards must be approved by the Director and shall be required to attend a training session conducted by the Airport Manager prior to gate assignment."

8.8 LIQUIDATED DAMAGES FOR FAILURE TO COMPLETE THE WORK OR PORTIONS OF THE WORK ON TIME: The General Provisions is hereby amended to include the following:

The schedule of liquidated damages provided in Section 8.8 of the General Provisions in these specifications shall be amended to include the following:

FIVE THOUSAND DOLLARS (\$5,000.00) per calendar day for failure to complete the contract within 696 calendar days.

FORTY-FIVE THOUSAND DOLLARS (\$45,000.00) per calendar day for failure to begin and end each phase on the dates noted below.

PHASE	BEGIN DATE	END DATE
2B	2/20/2025	6/11/2025
3B	6/12/2025	10/01/2025

NINE THOUSAND DOLLARS (\$9,000.00) per hour for delay in re-opening the runway for overnight closures required for Phases 1A, 2A, 3A, 4A, 4B, 4C, 4D, and 5.

8.20 LIMITATION OF OPERATIONS: is hereby added to the General Provisions:

"The following limitations shall be observed by the Contractor when operating within 75 feet from the edge of any taxiway.

General - The Contractor shall schedule his operations to minimize interference with the movement of aircraft or passengers as may be required by the Engineer. The Contractor shall be responsible to alert all of his personnel to the location of power and signal cables installed for the operation of the airport. The Contractor shall control his operations in a manner to preclude any possible damage to those cables. Utility companies shall be notified by the Contractor one week before

commencement of work. The Contractor shall give notice to the Engineer in writing, at least 168 hours before operating within 75 feet from the edge of any taxiway and the Engineer will assure himself that the Airport Management personnel are notified in sufficient time to publish the warning (NOTAM). The Contractor shall immediately repair any damages to the existing perimeter fence to prevent inadvertent entry to the Airport Operation Area (AOA).

Work in Vicinity of Runways and Taxiways in Use - Under the terms of this contract, it is intended that work shall be completed without disturbing the paved surface of existing runways and taxiways, unless shown otherwise on the plans.

Aircraft traffic shall not be interrupted. The Contractor shall schedule to work within 75 feet of the taxiway as directed by the Airport Management. No ruts, holes, or open trenches of 3 inches or more in depth and no objects or material 3 inches or more in height shall be permitted within the safety area when the airfield is in operation in conformance to Federal Aviation Regulation Part 139. The Contractor is also informed that Airport Zoning Regulations dictate that a 'clear zone' be maintained 500 feet on each side of an active runway, to be known as a hazardous area. The Contractor shall comply with all regulations governing ground operations within hazardous areas. The following FAA Advisory Circulars or later versions and FAA Regulations specify these requirements.

AC 150/5210-5D Painting, Marking, and Lighting Vehicles Used on an Airport, dated April 2010

AC 150/5340-1M Standards for Airport Markings, dated May 2019

AC 150/5370-2G Operational Safety on Airports During Construction, dated December 2017

FAA Regulations Objects Affecting Navigable Airspace Part 77

The Contractor shall keep all personnel and equipment off the areas not specifically designated for work under this Contract. At all times when the Contractor's equipment is not in use, the equipment shall be moved outside the hazardous areas to an area designated by the Engineer. Under no condition shall equipment be parked or material stored within the hazardous areas.

Failure on the part of the Contractor to abide by the above will result in suspension of work.

Authority of Control Tower Personnel - With the exception of actual construction methods, the airport control tower personnel will have full authority to control the Contractor's movements within the existing taxiway. When required, the Contractor shall maintain a constant radio vigil within all work areas and in addition shall keep at least one flagman on duty with the radio man. When notified by the control tower to temporarily halt operations, it shall be the duty of the flagman, through the use of appropriate methods (lighted flares shall not be used under any circumstances), to notify all operators of equipment and other personnel to cease work and move men and equipment off of hazardous areas. Contractor shall provide, at his own expense, the necessary radio and equipment including a radio equipped mobile vehicle to maintain contact with control tower personnel at all times during job performance. A transceiver operating at a frequency designated by the Engineer to communicate with the Control Tower.

Marking of Hazardous Areas - The Engineer will designate areas that are hazardous for aircraft. The Contractor shall provide red blinker lights spaced not more than 50 feet apart around all hazardous areas and areas of work within 75 feet of any taxiway. Such systems shall be subject to approval by the Engineer. The Contractor shall have personnel on call 24 hours per day for the emergency maintenance of hazard markings.

The Contractor shall provide red flags not less than 20 inches square in addition to the red blinker lights. When danger flags are made of fabric, a wire stiffener shall be used to hold the flags in an

extended position. Flags shall be so mounted that they do not produce a hazard. The red danger flags shall be spaced not more than 50 feet apart around all areas of work within 75 feet of any taxiway.

All systems proposed by the Contractor for lighting and barricading shall be submitted to the Engineer for review prior to installation. The Contractor shall install all flags, lighting and barricades as required by the Engineer. Such systems shall be subject to approval by the Engineer.

Storage of Equipment and Materials - At the end of each working shift, all of the Contractor's equipment shall be withdrawn to an area designated by the Engineer. The Contractor shall park all equipment in an orderly fashion and place a sufficient number of red flasher lights to identify these areas. Materials stored within the airport shall be so placed and the work shall, at all times, be so conducted as to cause no greater obstruction to the air and ground traffic than is considered necessary by the Engineer. No runways, taxiways or roadways shall be closed or opened, except by permission of the Engineer.

Blasting Operations - The Contractor shall notify the Engineer at least three (3) days before performing blasting operations as to the extent and timing of such operations, so that the Control Tower and other concerned parties can be informed.

Utilities - The Contractor shall provide for the protection of all utilities from damages in areas to be traversed by his vehicles and equipment. If required, buried cables and utility lines shall be protected by mounding earth over the cables or by any other method approved by the Engineer.

The Contractor shall notify representatives of the owner, agencies, and other affected organizations at least 48 hours prior to working in any area containing the facilities of these organizations.

Failure to notify the owning organization will prevent authorization to work in a specific area.

Archaeological Features - Any archaeological features such as petroglyphs, burial sites, and artifacts discovered or unearthed during the performance of the work shall immediately be brought to the attention of the Engineer and all work that would damage or destroy these features shall be discontinued. The Engineer will decide, after proper investigation, to salvage or abandon such artifacts."

8.21 OPERATION OF CONTRACTOR'S MOTOR VEHICLE AND PERSONNEL IN RESTRICTED AIR OPERATIONS AND MOVEMENT AREAS is hereby added to the General Provisions:

"The contractor shall conform with all sections of the "State of Hawaii, Department of Transportation, Airports, Contractor's Training Guide" pertaining to access and operation in the Airport Operation Area (AOA) hereinafter described as follows:

A. Motor Vehicles in Airport Operation Area

For safety reasons, the operation of motor vehicles in the AOA must conform with all applicable State Airport rules and regulations."

B. Motor Vehicle Access Permit

Each motor vehicle operated in the AOA is required to:

1. Meet all State licensing registration and safety requirements and be specifically licensed for operation in the AOA.
2. Meet all insurance requirements.
3. Be restricted to operation by those persons qualified to drive the vehicle and in possession of a current Ramp Driver's License and applicable Motor Vehicle

Operator's License.

C. The operators of motor vehicles in the AOA shall be responsible for meeting the following insurance requirements.

1. Licensed Vehicles

As a condition for authorization to enter the AOA, the Contractor shall provide evidence of vehicle liability insurance in the form of a Certificate of Insurance issued by an authorized insurance carrier. Automobile Liability and general Liability (combined single limit, Bodily Injury and Property Damage, per occurrence) shall be required in the applicable minimum limits specified below:

a. Daniel K. Inouye International Airport

(1) Standard AOA clearance....\$5,000,000

(2) Limited AOA clearance\$1,000,000 Limited AOA clearance is defined as operations restricted to Diamond head and Ewa Concourses second level roadways and connecting third level main terminal roadway only, with entry and exit via Security Access Point "C" (Primary) and Access Point "A" (Secondary)

b. Other Airports

Standard AOA clearance.....\$1,000,000

Standard AOA clearance is defined as any portion of a public Airport from which the public is restricted by fences or appropriate signs and no leased or demised to anyone for exclusive use and shall include runways, taxiways, all ramp and apron areas, aircraft parking and storage areas, fuel storage areas, maintenance areas, and any other area of a public Airport used or intended to be used for landing, takeoff, or surface maneuvering of aircraft or used for embarkation or debarkation of passengers.

2. Unlicensed Vehicles

Airport Liability (or General Liability) shall be required in the applicable minimum limits specified below:

a. Daniel K. Inouye International Airport, Kahului Airport and Ellison Onizuka Kona International Airport at Keahole

AOA clearance.....\$5,000,000

b. All other Airports

AOA clearance.....\$1,000,000

3. Specifically name the State of Hawaii as additionally insured.

4. Indicate that the Airport Engineer will be provided with a 30-day written prior notice of policy cancellation or material change in coverage or conditions.

D. Operator's Permit

1. No person shall operate a motor vehicle on the AOA unless he holds and carries on his person a current Airport Motor Vehicle operator's permit issued by the

State of Hawaii, Department of Transportation, Airports.

2. Operator's permits will only be issued to persons who apply through the Airport District Security Office and pass a written exam covering those portions of the Airport Rules and Regulation relating to the operation of vehicles in Airport Operations Areas.

E. Authorized Vehicles

1. Only vehicles considered operationally safe and necessary for the performance of this contract may be allowed to operate in the AOA.
2. All motor vehicles must be painted in such a manner so as to be easily identifiable and must carry the Contractor's name on each side. These signs may be of a temporary nature applied to the side windows or doors.

The lettering shall be in bold characters of a minimum of four (4) inches in height and one and one-half (1-1/2) inches in widths, the height of logos should be a minimum of six (6) inches.

3. The Contractor's operations on, over, across, and/or immediately adjacent to any runway and/or taxiway at a towered airport shall require the use of two-way radio communication. The Contractor shall obtain the necessary equipment at his own expense.
4. No person shall operate a motor vehicle on the AOA unless he holds and carries on his person a current Motor Vehicle Operator's Permit issued by the Airport Manager.
 - a. The Motor Vehicle Operator's Permit will be issued only to persons who apply through the Airport Security Section and pass a written exam covering those portions of the Airport Rules and Regulations relating to the operation of vehicles in the AOA.
 - b. Permits issued may be suspended or revoked for cause at any time by the Airports.

F. Airport Operation Area Construction Pass

1. Issuance of Airport Operation Area (AOA) Construction Passes shall be limited to contractors, subcontractors, companies, organizations, individuals engaged in authorized and approved construction activity which requires a continuing need for entry into the AOA or Airfield Movement Areas Request letters for such passes must be made to the Airport District Manager's Office in accordance with the Contractors Training Guide or applicable District requirements.
2. As a condition for security area clearance, applicants must comply with Transportation Security Regulation 1542 which requires a ten-year background Criminal History Records Check for those individuals employed under this contract.

G. Access to Movement Areas

1. Movement areas shall mean all of the runways and taxiways of the Airport which are utilized for taxiing, takeoff, and landing of aircraft.
 - a. Any vehicle which requires access to the movement area shall be equipped with operational radio equipment capable of positive two-way contact with Tower/Ground Control.
 - b. Operators of vehicles in movement areas must possess knowledge and familiarity with restricted and airfield movement areas, operational rules, regulations, and procedures, or be under direct escort by individuals meeting all of the above requirements.
2. Vehicle Operations on Movement Areas
 - a. No vehicle shall proceed across any runway unless specifically cleared by Tower/Ground Control.
 - b. The operator of a vehicle in the movement area shall not leave his vehicle unless continuous radio contact is maintained with the Tower/Ground Control while he is away from his vehicle.
 - c. Any vehicle proceeding onto the movement area between the hours of sunset and sunrise shall be equipped with an overhead flashing light which is visible for one (1) mile, unless such vehicle is being escorted by another vehicle so equipped.
 - d. All vehicles operated on the movement area between sunrise and sunset except those being escorted, shall operate an overhead amber or red flashing beacon visible for at least one (1) mile; or display a flag at least three (3) feet square with orange and white checkered squares of not less than one (1) foot on each side.

H. Runway and Taxiway Closure

1. Requests for runway or taxiway closures, or for any work which affect operational conditions at the airport must be made in writing through the Airport Engineering Branch.
2. Temporarily closed runways require placement of a lighted "X" runway closure marker on top of the runway identification numerals at both ends of the closed runway.
3. Taxiway closures require placement of barricades with alternate orange and white markings at each end of the closed taxiway segment. Barricades must be supplemented with flashing red lights. The intensity of the lights and spacing for barricades, and lights must adequately define and delineate the hazardous area.

I. Gate Guards Furnished by Contractors

1. If a contractor is permitted by the airport to maintain operational control of an AOA Access Gate, entry through such gate shall be controlled by the posting of a gate guard.

- a. Written instruction will be provided, outlining the guard's duties to enforce those requirements and provisions prescribed by the airport's security program to include all personnel and vehicle entry and access requirements.
- b. Procedures will be established to identify the actions which will be undertaken by the guard in calling for assistance.
- c. An approved emergency communications procedure will be established.

J. Compliance

- 1. The contractor shall comply with all regulations and rules governing the Air Operations Areas during construction, as specified in the following or later versions:
 - a. Hawaii Revised Statutes, Title 19, Administrative Rules for Public Airports.
 - b. Federal Aviation Administration Advisory Circular AC 150/5340-1, Standards for Airport Markings; AC 150/5370-2, Operational Safety on Airports During Constructions.

K. Enforcement Authorization

Act 21, Section 1, Section 261-17(a), HRS; Federal Aviation Administration Regulations, Part 139, Part 107.

L. Right of Rejection or Revocation

The State of Hawaii, Airports, reserves the right to withhold, deny or revoke any airport security clearance, licenses or permits to any individual or organization who fails to meet the prescribed or required access area clearance criteria to include background investigation information, or fails to observe or comply with established rules, regulations, and directives.

It should be clearly understood that such denial or revocation is based solely on airport security or safety considerations and does not in any way constitute a determination by the State with regard to private employment by any individual or organization."

-----END OF SECTION-----

PART II – TECHNICAL PROVISIONS

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SECTION 01351 – ESCROW PROPOSAL DOCUMENTATION (EPD)

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. The General Provisions, Special Provisions, and General Requirements of the Specifications, apply to the work specified in this section.

1.2 ESCROW PROPOSAL DOCUMENTATION (EPD)

- A. **General:** Each bidder, including pricing data of all subcontractors and suppliers, shall submit the EPD spread sheets in a sealed container, separate from the envelope containing the proposal, which shall contain the summary information and calculations used to determine the proposal for this project. Within five (5) calendar days after the bid date, the bidders shall submit to the Contracts Officer all supporting information and calculations used by the bidders to determine the proposal for this project. This documentation is hereafter referred to as "Escrow Proposal Documentation" or EPD.

The EPD of the successful bidder will be held in escrow for the duration of the contract and will be used solely for the purpose of determining the Contractor's proposal concept, if necessary, for price adjustments as provided in the contract or to resolve any claim by the Contractor. The EPD of all other bidders, except the three lowest bidders, will be returned unopened after determination of low bidder is made. If the contract is not awarded to the lowest bidder, the EPD of the next lowest bidder will be processed as specified herein. Upon award of the contract to the lowest bidder, the remaining bidders' EPD will be returned.

The Department stipulates and expressly acknowledges that the EPD as defined herein, constitutes trade secrets. This acknowledgment is based on the Department's express understanding that the information contained in the EPD is not known outside bidder's business, is known only to a limited extent and by a limited number of employees of the bidder, is safeguarded while in bidder's possession, is extremely valuable to bidder and could be extremely valuable to bidder's competitors by virtue of it reflecting bidder's construction strategies, assumptions and intended means, methods and techniques of construction. The Department further acknowledges that bidder expended substantial sums of money in developing the information included in the EPD and further acknowledges that it would be difficult for a competitor to replicate the information contained therein and are being provided to the Department only because it is an express prerequisite to award of the contract. The Department further acknowledges that the EPD includes a compilation of information used in bidder's business, intended to give bidder an opportunity to obtain an advantage over competitors who do not know of or use the contents of the documentation. The Department further agrees to safeguard the EPD against disclosure to the fullest extent permitted by law.

The apparent successful bidder agrees, as a condition of award of the contract, that the EPD constitutes all the information used in the preparation of his bid, and that no other bid preparation information shall be considered in evaluating disputes or claims.

- B. Format: Bidders are encouraged to submit the EPD in their usual cost estimating format; a standard format is not required. It is not the intention of this specification to cause the bidder extra work during the preparation of his proposal, but to ensure that the documentation will be adequate to enable complete understanding and proper interpretation for the intended use.

The EPD will clearly itemize the costs for each pay item. The EPD shall be in English. Each pay item shall be broken down into components small enough to allow a detailed cost estimate. Costs allocated to each component shall be broken down into the bidder's usual estimate categories such as direct labor, repair labor, equipment parts and supplies, expendable materials, permanent materials, and subcontractor costs as appropriate. Plant and equipment and indirect cost allocations shall be made to each bid item as appropriate and broken down in the bidder's usual format. All costs shall be identified.

The EPD shall include quantity takeoffs, calculations of rates of production and progress, copies and quotes from subcontractors and suppliers, and memoranda, narratives, reference material and all other information used by the bidder to arrive at all of the prices contained in his proposal.

- C. Submittal: The EPD shall be submitted in a sealed container, separate from the envelope containing the proposal, and shall be clearly marked with the bidder's name, date of submittal, project name and 'ESCROW PROPOSAL DOCUMENTATION.' The EPD shall be accompanied with an affidavit, in the form following this section.

Failure to submit the EPD will be cause for rejection of the proposal.

- D. Examination: The EPD of the apparent successful bidder will be examined by the Department in the bidder's presence prior to award of the contract. At least one member of the bidder's staff who is knowledgeable in how the bid was prepared shall be present. This examination is to ensure that the EPD is complete and does not constitute approval of proposed construction methods, estimating assumptions, or interpretations of contract documents. Incomplete and/or missing data shall be promptly supplied, but in all cases prior to award of contract. Examination does not alter any condition or term of the contract. After examination, the EPD will be resealed, in the presence of both the Department and the bidder, and stored. Receipt of the EPD will be acknowledged by the Department by return of the bidder of the countersigned affidavit as specified below.

- E. Storage: The EPD is, and shall remain, the property of the Contractor, subject to use as provided herein. The EPD shall be placed in escrow, during the life of the contract, in a banking institution suggested by the Department and acceptable to the Contractor. The cost of storage will be paid by the Contractor.

- F. Review: The EPD will be initially reviewed by the Department in order to establish pay item amounts in the Schedule of Values for progress payments, and may be reviewed at any other time deemed necessary by the Department or the Contractor, in conjunction with settling disputes, claims or change orders. Review of the EPD is subject to the following conditions:
1. As trade secrets as described herein, the EPD is proprietary and confidential and shall be treated as such.
 2. The Department and the Contractor shall each designate in writing three representatives who are authorized to review the EPD on their behalf. When considered necessary by both the Department and the Contractor, members of the Disputes Review Board may review the EPD.
 3. The Department and the Contractor shall each designate in writing a representative to receive notice of review of the EPD on their behalf.
 4. Prior to reviewing the EPD, 24 hours written notice shall be given by the Department or the Contractor to the other party, so that the review can be witnessed by the other party.
 5. No representative of the Department or the Contractor shall be allowed to gain access to the EPD without an authorized representative of the other party being present.
 6. Following each review, the EPD will be returned to the escrow institution.
- G. Return: The EPD will be returned to the Contractor after all claims and disputes have been resolved, and final payment on the contract has been made and accepted.

ESCROW PROPOSAL DOCUMENTATION AFFIDAVIT

THE UNDERSIGNED HEREBY CERTIFIES THAT THE ESCROW PROPOSAL DOCUMENTATION CONTAINED HEREIN CONTAINS ALL OF THE INFORMATION WHICH WAS USED TO DEVELOP THE PROPOSAL AND THAT THE ESCROW PROPOSAL DOCUMENTATION IS ACCURATE, CORRECT AND COMPLETE.

BY:

TITLE:

FIRM: _____

DATE: _____

COUNTERSIGNED BY THE DEPARTMENT AFTER EXAMINATION OF ESCROW PROPOSAL DOCUMENTATION.

BY: _____

TITLE: _____

DATE:

END OF SECTION

SECTION 16101 – AIRFIELD ELECTRICAL DEMOLITION AND REMOVAL

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. The General Provision of the contract, including the General Provisions for Construction Projects (2016), Special Provisions, and General Requirements of the Specifications, apply to the work specified in this section.

1.2 DESCRIPTION OF WORK

- A. The airfield electrical work to be done under this contract shall include the furnishing of all supervision, labor, materials, tools, equipment, and incidentals necessary to remove all of the airfield electrical equipment both above-ground and underground and all other electrical work as described herein and shown on the drawings. This specification covers removal of NAVAID equipment, base cans, conduit, signage, foundations, cable, and all other electrical equipment as defined in the drawings.
- B. Work shall be in accordance with current edition of Federal Aviation Administration (FAA) Advisory Circular (AC) No. 150-5370-10, “Standards for Specifying Construction of Airports,” as modified herein, other FAA Advisory Circulars and Specifications referred to herein, and other requirements as specified herein.
- C. The electrical work shall comply with the latest adopted editions, codes and standards to this Contract as follows:
 - 1. ANSI C2, National Electrical Safety Code
 - 2. FAA Advisory Circulars
 - 3. FAA Specifications including FAA-STD-019b, FAA-C-1217 and FAA-C-1391
 - 4. NEC, National Electric Code (NFPA 70)
 - 5. NECA, Standard for Installation
 - 6. NEMA, Standard for Materials and Products
 - 7. NFPA, No. 101, Life Safety Code
 - 8. UL, Underwriters Laboratories
- D. All work shall be performed in strict accordance with these contract specifications, drawings, and any instructions that may be furnished by the RPR during execution of the work to aid in interpretations of said drawings and specifications.
- E. All material furnished for this project shall be listed by Underwriters Laboratories wherever UL has a listing standard for that material.

1.3 RELATED WORK SPECIFIED ELSEWHERE (NOT USED)

1.4 REFERENCES (NOT USED)

1.5 SUBMITTALS (NOT USED)

PART 2 - PRODUCTS (NOT USED)

PART 3 - EXECUTION

3.1 VERIFICATION OF EXISTING ELECTRICAL EQUIPMENT

A. Contractor shall verify that any existing electrical equipment to be removed and demolished is not energized and has been properly locked-out and tagged-out at the source. Note; some of the equipment may be owned by the FAA and may require special coordination with FAA Technical Operations Support Services.

3.2 CONFLICTS BETWEEN DOCUMENTS

A. Prospective contractors shall, as part of their proposals, enumerate, identify and list conflicts that they discover to exist within the contract documents and/or between those documents and the rules, regulations, standards.

3.3 TEMPORARY LIGHTING AND CIRCUITS

A. Contractor shall coordinate with Operations and Maintenance at the end of each work shift to verify that all temporary airfield lighting circuits are operational. Contractor shall provide all labor and material for this work.

B. Contractor shall provide and maintain on hand sufficient equipment required to provide temporary lighting and circuit extensions. This includes, but is not limited to, fixtures, transformers, bases, two-inch conduit, L-824 cable and L-823 connectors. These items will not be available from KOA Maintenance.

3.4 CONFIRMATION OF EXISTING CIRCUITING AND ROUTING

A. Prior to the installation of any material and equipment and within 30 days of the administrative notice to proceed date, the Contractor shall trace routing for each circuit and cable inside the project work area. This includes opening and pumping out existing handholes/manholes to ascertain size and number of conduits and cable routing. Contractor shall submit a shop drawing prior to any demolition that provides the following minimum information:

1. Butterflies of every existing handhole/manhole that shows the number and type of conduits.
2. Markups of existing area plans detailing the actual markings of circuits including FAA, power, airfield lighting, and control.
3. Contractor shall perform utility tracing to confirm that circuits believed to be de-energized are not operational.

3.5 OWNERSHIP OF REMOVED ELECTRICAL EQUIPMENT

A. All spoils, equipment, and material shall become the property of the Contractor and disposed off airport property in compliance with State of Hawaii guidelines unless specifically stated otherwise in the construction drawings as equipment to be reinstalled.

3.6 SALVAGEABLE ITEMS

- A. Equipment, materials and components designated to be removed and reinstalled will remain the Department of Airport's property at the Airport designated salvage area. These items shall be carefully removed and shall be delivered to the Airport's maintenance yard and stockpiled in a neat orderly fashion as directed by the RPR. If it is determined that through the Contractor's operations of removing and handling, these items are being damaged, the RPR reserves the right to withhold payment from the Contractor for compensation of these items.

3.7 CABLE REMOVAL

- A. All the existing cables to be removed shall become the property of the Contractor to be promptly removed from the airport property. Temporary storage of these items on airport property shall be subject to the approval of the RPR.

3.8 CONDUIT, REMOVAL AND ABANDONMENT

- A. Existing conduit shall be removed as shown on the Drawings, or as directed by the RPR. Removed items shall become the property of the Contractor and shall be promptly removed from airport property.

Conduit designated to be abandoned in place shall be capped on each end as approved by the RPR with existing cable to be removed. Where conduit comes to the surface, the conduit shall be cut back to minimum one (1) foot below ground, final grade, and capped.

3.9 DUCTBANK, REMOVAL AND ABANDONMENT

- A. Existing ductbank shall be removed as shown on the Drawings, or as directed by the RPR. Removed items shall become the property of the Contractor and shall be properly removed and disposed off Airport property.

3.10 REMOVAL OF EXISTING BASE CANS

- A. Base Cans shall be removed as detailed on the construction drawings. Remove base cans prior to placement of new pavement and backfill with concrete in accordance with Section 02610, Concrete for Miscellaneous Structures up to grade of new subbase.

3.11 REMOVAL AND SALVAGE OF EXISTING LIGHT FIXTURES AND/OR COVER PLATES

- A. Prior to performing any demolition work, the Contractor shall perform a visual inspection in conjunction with the Airport to determine if any of the taxiway edge light fixtures are missing, damaged, or have a burned-out lamp. This visual inspection shall be performed during the day and at night with the light fixtures powered on, and the Contractor shall provide the RPR with a written document detailing the deficiencies found. If any of the light fixtures are damaged or lost during the construction project,

the Contractor shall replace the damaged or lost light fixture with a new, equal or approved equal light fixture at the Contractor's own expense.

- B. Light fixtures, isolation transformers, connectors and cable shall be removed as indicated on the drawings. L-868 heavy duty, 3/4" thick cover plates shall be installed on all base cans.
- C. Light Fixtures that require removal shall be salvaged and delivered to the Owner's Department of Maintenance at Airport'. For these fixtures, the Contractor shall protect the leads by curling them and then taping them to the bottom side of the fixture housing. The fixture shall be placed inside a circular plastic tube that is the diameter (circumference) of the light and approximately 3" tall.

PART 4 - MEASUREMENT AND PAYMENT

4.1 DEMOLITION AND REMOVAL OF AIRPORT ELECTRICAL ITEMS

- A. Demolition and Removal of Electrical Items shall be measured per lump sum. This work includes but not limited to the following:
 - 1. This bid item covers removal of electrical items as detailed on E100 series and are outside the pavement demolition limits. Pavement demolition limits are defined as any demolition of asphalt full strength pavement, asphalt shoulder, or concrete as shown on the civil drawings. Note; the cost for the removal of electrical items as detailed on the E100 series drawings and are inside the pavement demolition limits shall be bid under the appropriate civil bid item and are not included in this bid item.
 - 2. Utility Detection, Survey, and Tracing of all conduits and cables. Including providing shop drawing that defines the existing routing. Included in this work is the opening and closing of the existing manholes/handholes required to trace the circuits regardless of the number of times contractor enters structure.

4.2 DEMOLITION AND REMOVAL OF FAA MALSR ITEMS

- A. Demolition and Removal of FAA MALSR Items shall be measured per lump sum. This bid item includes but not limited to the demolition and removal of all FAA MALSR electrical items as detailed in the electrical demolition series of drawings and not bid separately. This includes all items including but not limited to the MALSR threshold bar to Stations 2 through and inclusive of Station 24 and Distribution Panel Cabinet. This work also includes but not limited to the following:
 - 1. Removal of MALSR/FAA conduit, ductbank, and cable. Payment for the handling and disposal of piping and materials shall be included in this bid item.
 - 2. Removal of existing MALSR light poles, base cans, handholes, and abandoned concrete structures as detailed in demolition drawings.

4.3 BASIS OF PAYMENT

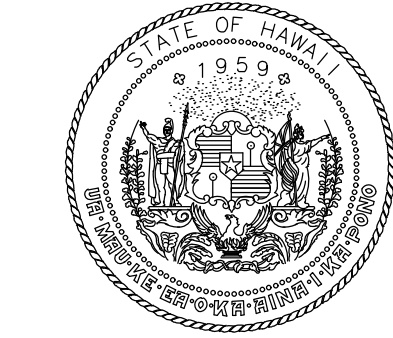
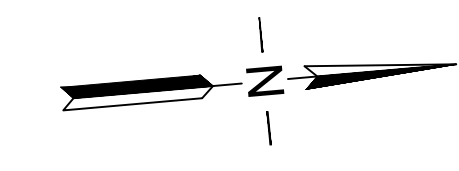
A. Payment will be made under:

<u>Item No.</u>	<u>Description</u>	<u>Unit</u>
16101.1	Demolition & Removal of Airport Electrical Items	Lump Sum
16101.2	Demolition & Removal of FAA MALSR Items	Lump Sum

PART 5 - ATTACHMENTS (NOT USED)

-----END OF SECTION 16101-----

ID	Task Name	Start	Finish	Duration	2024																								
					Qtr 2, 2024	Qtr 3, 2024	Qtr 4, 2024	Qtr 1, 2025	Qtr 2, 2025	Qtr 3, 2025	Qtr 4, 2025	Qtr 1, 2026																	
0	KOA 17-35 REHABILITATION	Mon 7/15/24	Sun 4/12/26	636 days	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr
1	Preconstruction	Mon 7/15/24	Tue 11/12/24	120 days	Preconstruction																								
2	Phase 1A - Temp PAPI Installation	Tue 11/12/24	Sat 11/23/24	11 days	Phase 1A - Temp PAPI Installation																								
3	Runway Closure Moratorium (Thanksgiving)	Sat 11/23/24	Mon 12/2/24	9 days	Runway Closure Moratorium (Thanksgiving)																								
4	Phase 1A - Temp PAPI Installation	Mon 12/2/24	Wed 12/18/24	16 days	Phase 1A - Temp PAPI Installation																								
5	Runway Closure Moratorium (Christmas)	Wed 12/18/24	Wed 1/1/25	14 days	Runway Closure Moratorium (Christmas)																								
6	Phase 1A - Temp PAPI Installation	Thu 1/2/25	Sun 1/5/25	3 days	Phase 1A - Temp PAPI Installation																								
7	Phase 1B - Flight Check Period	Sun 1/5/25	Wed 2/19/25	45 days	Phase 1B - Flight Check Period																								
8	Phase 2A - Shorten Runway 35 End	Wed 2/19/25	Thu 2/20/25	1 day	Phase 2A - Shorten Runway 35 End																								
9	Phase 2B - Runway 35 End Construction	Thu 2/20/25	Wed 6/11/25	111 days	Phase 2B - Runway 35 End Construction																								
10	Phase 3A - Shorten Runway 17 End	Wed 6/11/25	Thu 6/12/25	1 day	Phase 3A - Shorten Runway 17 End																								
11	Phase 3B - Runway 17 End Construction	Thu 6/12/25	Wed 10/1/25	111 days	Phase 3B - Runway 17 End Construction																								
12	Phase 4A - Restore Runway Full Length	Wed 10/1/25	Thu 10/2/25	1 day	Phase 4A - Restore Runway Full Length																								
13	Phase 4B - Middle Runway Construction	Thu 10/2/25	Sun 11/16/25	45 days	Phase 4B - Middle Runway Construction																								
14	Phase 4C - Middle Runway Construction	Sun 11/16/25	Thu 11/20/25	4 days	Phase 4C - Middle Runway Construction																								
15	Runway Closure Moratorium (Thanksgiving)	Sat 11/22/25	Mon 12/1/25	9 days	Runway Closure Moratorium (Thanksgiving)																								
16	Phase 4D - Middle Runway Construction	Mon 12/1/25	Tue 12/16/25	15 days	Phase 4D - Middle Runway Construction																								
17	Runway Closure Moratorium (Christmas)	Wed 12/17/25	Thu 1/1/26	15 days	Runway Closure Moratorium (Christmas)																								
18	Phase 4D - Middle Runway Construction	Thu 1/1/26	Sat 1/31/26	30 days	Phase 4D - Middle Runway Construction																								
19	Phase 5 - Grooving and Marking	Sat 1/31/26	Sun 4/19/26	78 days	Phase 5 - Grooving and Marking																								



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS



Martin A. Clevon
4/30/24
Licensed Expiration Date

This work was prepared by me or under my supervision.

DSGN.	DRWN.	CHKD.	APPD.
AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
1	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

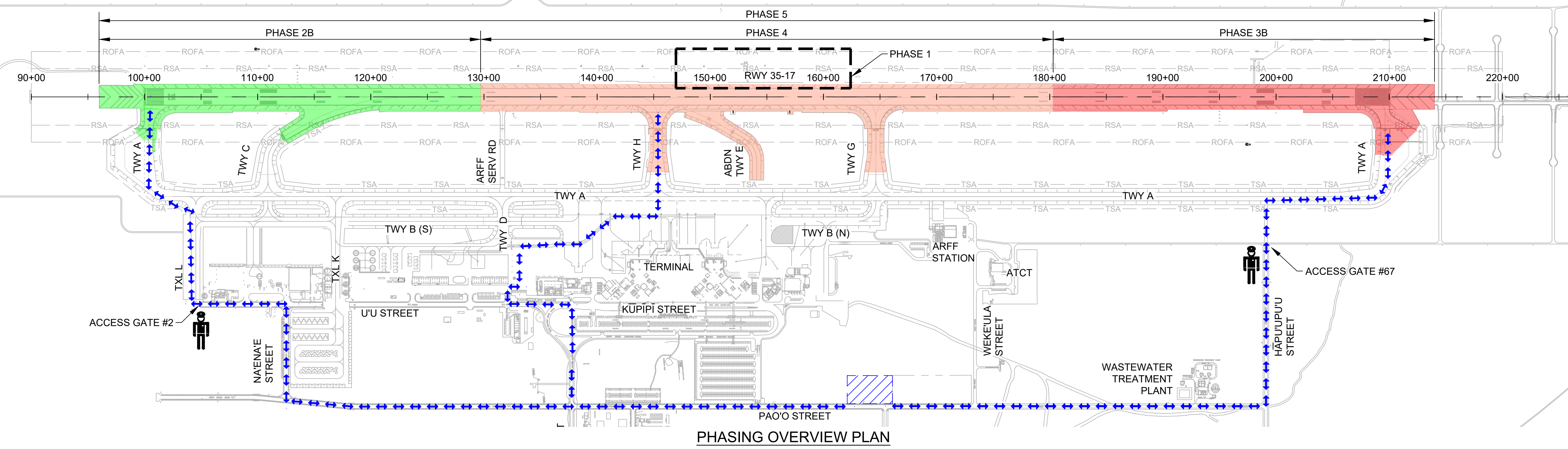
PROJECT NO.:

AH2021-16

SHEET TITLE:

PHASING AND BARRICADE PLAN OVERVIEW

DATE :	DWG. NO.
09/2023	G-101
SHEET :	6 OF 190 SHEETS



LEGEND:

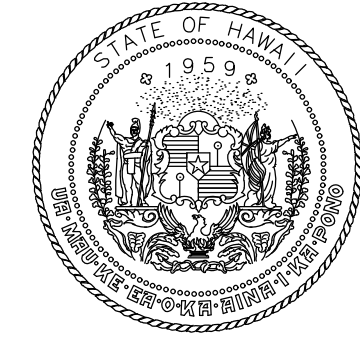
- PHASE 2
- PHASE 3
- PHASE 4
- CONTRACTOR STAGING AREA
- CONTRACTOR HAUL ROUTE (2 WAY)
- SECURITY GUARD (SEE SPEC 01565 SECURITY MEASURES)
- ROFA — RUNWAY OBJECT FREE AREA
- RSA — RUNWAY SAFETY AREA
- TOFA — TAXIWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA

CONSTRUCTION PHASING GENERAL NOTES:

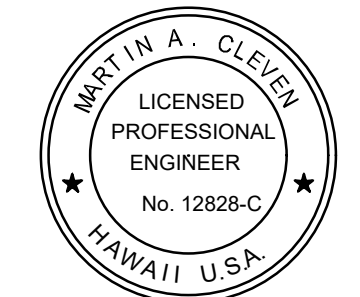
- ALL AOA AIRCRAFT PAVEMENT SURFACES SHALL BE OPEN TO AIRCRAFT OPERATIONS AT ALL TIMES OTHER THAN DESIGNATED CLOSURES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEPLOYING, MAINTAINING, AND REMOVING THE LIGHTED "X" SIGNALS IN ACCORDANCE WITH THE CONSTRUCTION PHASING PLANS.
- PRIOR TO REOPENING THE RUNWAY, THE CONTRACTOR SHALL REPAVE ALL MILLED SURFACES AND ENSURE NO DROPOFFS ARE PRESENT.
- THERE SHALL BE NO GREATER THAN 3" DROP WITHIN THE RUNWAY OR TAXIWAY SAFETY AREAS.



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STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS



Mark A. Clevon
4/30/24
Licensed Expiration Date

This work was prepared by me or under my supervision.

DSGN.	DRWN.	CHKD.	APPD.
AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
12/04/23		ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

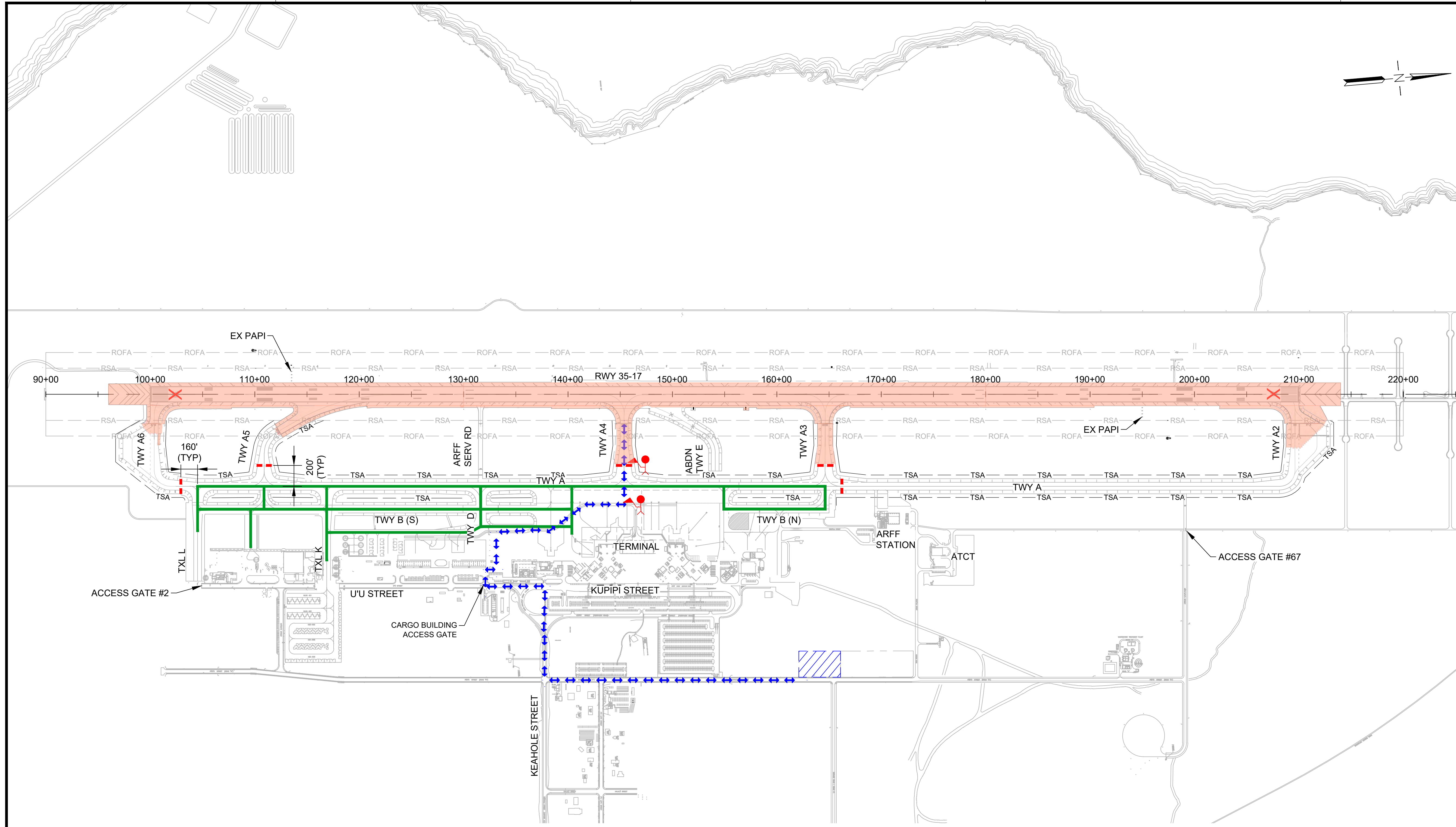
PROJECT NO.:

AH2021-16

SHEET TITLE:

**PHASING AND BARRICADE PLAN
PHASE 5**

DATE :	DWG. NO.
09/2023	G-123
SHEET :	
28 OF 190 SHEETS	



LEGEND:

- WORK AREA
- AIRCRAFT TRAVEL ROUTE
- LOW-PROFILE BARRICADES (CONTINUOUS)
- LOW-PROFILE BARRICADES (WITH GAPS)
- LIGHTED CLOSED RUNWAY MARKER
- CONTRACTOR STAGING AREA
- CONTRACTOR HAUL ROUTE (2 WAY)
- BLAST FENCE BARRIER
- FLAGGERS

PHASING AND BARRICADE PLAN - PHASE 5

DESCRIPTION OF WORK:
RUNWAY GROOVING AND FINAL PAVEMENT MARKINGS

HOURS:
SUNDAY - THURSDAY: 0000-0600 (HST), 1000-1600 (UTC)

DURATION:
78 CALENDAR DAYS
ANTICIPATED DATES: 1/31/2026 TO 4/19/2026

- REQUIRED NOTAMS (WORK HOURS):**
1. RUNWAY 17-35 CLOSED
 2. TAXIWAY A6 CLOSED
 3. TAXIWAY A5 CLOSED
 4. TAXIWAY A3 CLOSED
 5. TAXIWAY A4 CLOSED
 6. TAXIWAY A2 CLOSED
 7. TAXIWAY A CLOSED, NORTH OF TAXIWAY A3

REQUIRED NOTAMS (NONWORK HOURS):

1. RUNWAY 17-35 UNGROOVED
2. TAXIWAY H RENAMED TAXIWAY A4
3. TAXIWAY G RENAMED TAXIWAY A3

NAVAID STATUS:

- RUNWAY 17 PAPI (FAA): OTS
- RUNWAY 17 PAPI (TEMP): N/A
- RUNWAY 17 LOC/DME: OTS
- RUNWAY 17 GS: OTS
- RUNWAY 35 PAPI (FAA): OTS
- RUNWAY 35 PAPI (TEMP): N/A

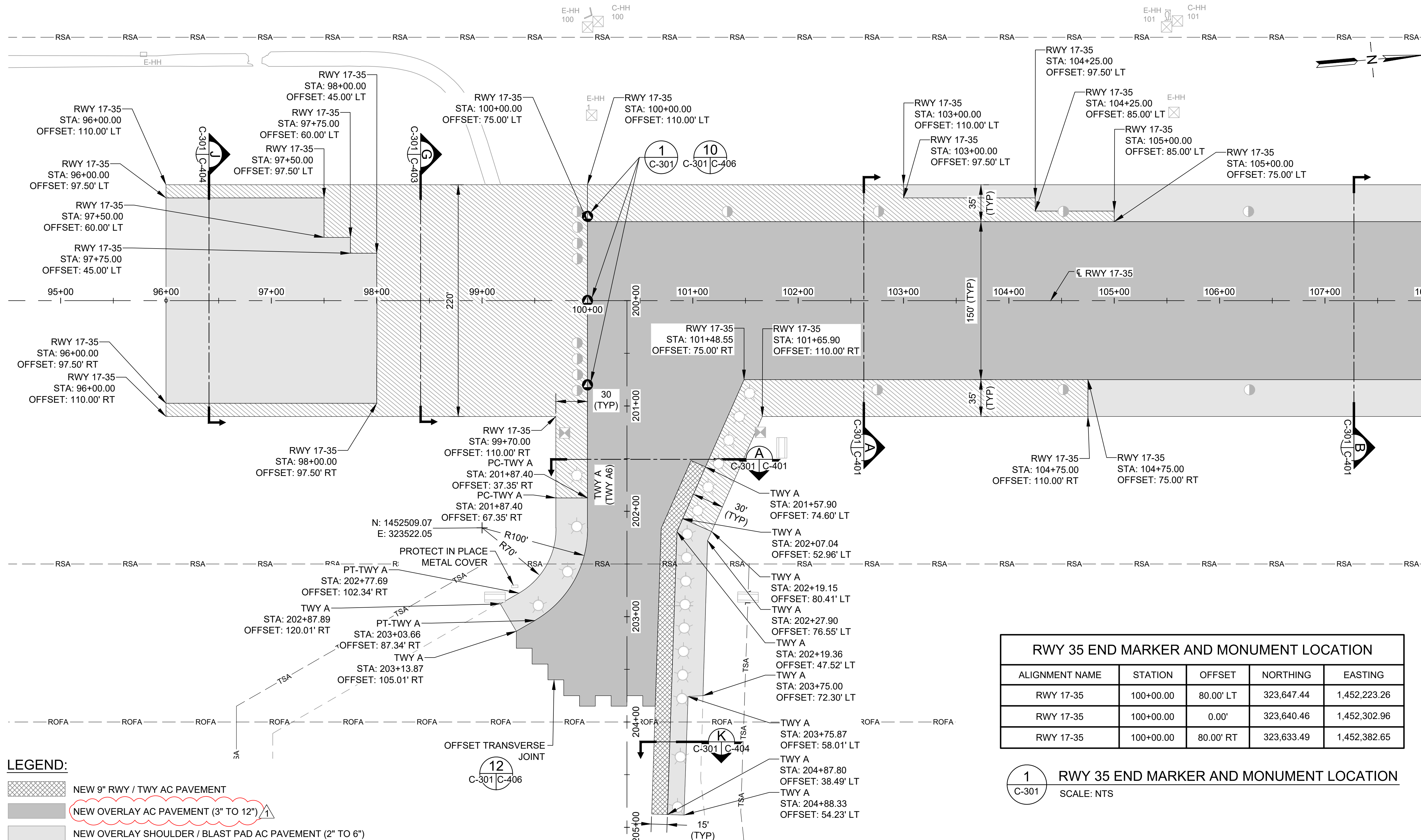
PHASING NOTES:

1. PRIOR TO OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE FOR AOC TO CONDUCT A RUNWAY INSPECTION.
2. SEE SPECIAL PROVISIONS FOR LIQUIDATED DAMAGES FOR FAILURE TO REOPEN THE RUNWAY AT 0600 HRS.

3. IN THE EVENT OF MEDICAL EMERGENCY, WITHIN 1-HR OF NOTIFICATION BY AIRPORT OPERATIONS, THE CONTRACTOR SHALL CLEAN THE RUNWAY OF ALL FOD AND DEBRIS, VACATE THE RSA, AND REOPEN THE RUNWAY TO ALLOW FOR ARRIVAL OR DEPARTURE OF EMERGENCY MEDEVAC AIRCRAFT.



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- LEGEND:**
- NEW 9" RWY / TWY AC PAVEMENT
 - NEW OVERLAY AC PAVEMENT (3" TO 12")
 - NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
 - NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
 - ELEVATED RUNWAY LIGHT
 - SEMI-FLUSH RUNWAY LIGHT
 - ELEVATED TAXIWAY LIGHT
 - MALS LIGHT
 - BASE CAN
 - NEW HANDHOLE
 - ADJUST EXISTING HANDHOLE
 - EXISTING HANDHOLE
 - SIGN
 - RUNWAY END MARKER AND MONUMENT

- NOTE:**
1. ALL MILLED RUNWAY SURFACES SHALL BE PAVED AND TRANSITION RAMP COMPLETED PRIOR TO REOPENING THE RUNWAY TO AIRCRAFT TRAFFIC.
 2. OFFSET TRANSVERSE JOINTS SHALL BE CONSTRUCTED AT THE INTERFACE WITH PAVEMENTS PLACED DURING PREVIOUS WORK SHIFTS. THE CONTRACTOR SHALL DEVELOP A PAVING PLAN THAT MINIMIZES OFFSET TRANSVERSE JOINTS.
 3. OFFSET TRANSVERSE JOINTS ON THE SURFACE COURSE SHALL BE OFFSET A MINIMUM OF 10' FROM TRANSVERSE JOINTS ON LOWER PAVING LIFTS.
 4. OFFSET TRANSVERSE JOINTS ARE NOT REQUIRED ON SHOULDERS.
 5. STEEL DRUM ROLLERS SHALL NOT BE PERMITTED ON CONCRETE, ONLY PNEUMATIC ROLLERS ARE PERMITTED ON CONCRETE. ANY DAMAGE TO CONCRETE SURFACES CAUSED BY ROLLERS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
 6. TRUCKS SHALL NOT TRAVERSE OVER CUT AND PREPARED JOINTS.
 7. VEHICLE TRAFFIC OVER TACK COAT SHALL BE KEPT TO A MINIMUM. TACKED SURFACES THAT HAVE BEEN CONTAMINATED SHALL BE CLEANED AND RE-TACKED AT THE DIRECTION OF THE ENGINEER AND AT THE CONTRACTOR'S EXPENSE.
 8. THE ALIGNMENT AND ELEVATION OF THE PAVER SHALL BE REGULATED FROM OUTSIDE REFERENCE LINES. THE CONTRACTOR'S LICENSED SURVEYOR SHALL SURVEY HMA AND CERTIFY TO ENGINEER THAT EVERY LOT MEETS THE CONTRACT GRADE TOLERANCES.
 9. AT THE CONCLUSION OF EACH WORK SHIFT, PRIOR TO OPENING THE RUNWAY OR TAXIWAY TO AIRCRAFT, THE CONTRACTOR SHALL ENSURE THAT NO DROP-OFFS GREATER THAN 3" ARE PRESENT.
 10. REFER TO ELECTRICAL DRAWINGS FOR ELECTRICAL STRUCTURES TO BE PROTECTED, ADJUSTED, REMOVED OR ABANDONED.
 11. SEE PHASING AND BARRICADE PLANS FOR APPROVED CONTRACTOR HAUL ROUTES.

RWY 35 END MARKER AND MONUMENT LOCATION				
ALIGNMENT NAME	STATION	OFFSET	NORTHING	EASTING
RWY 17-35	100+00.00	80.00' LT	323,647.44	1,452,223.26
RWY 17-35	100+00.00	0.00'	323,640.46	1,452,302.96
RWY 17-35	100+00.00	80.00' RT	323,633.49	1,452,382.65

1 RWY 35 END MARKER AND MONUMENT LOCATION
 C-301 SCALE: NTS

MATCHLINE, SEE DWG NO C-302

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS

MARKIN A. CLEVEN
LICENSED PROFESSIONAL ENGINEER
No. 12828-C
HAWAII, USA

4/30/24
Licensed Expiration Date

This work was prepared by me or under my supervision.

DSGN.	DRWN.	CHKD.	APPD.
AC	AT	TR	SH

KEY PLAN / NOTES:

NO.	DATE	REVISIONS
1	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
 SEPTEMBER 2023
 DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION
 AT
 ELLISON ONIZUKA
 KONA INTERNATIONAL AIRPORT AT KEAHOLE
 KAILUA-KONA, HAWAII

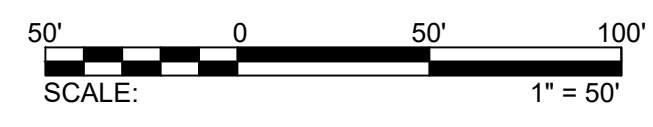
PROJECT NO.:

AH2021-16

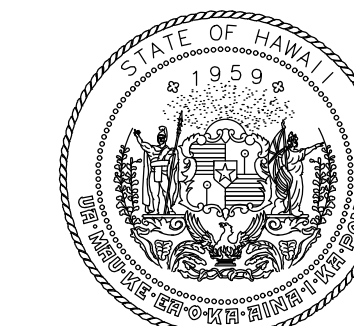
SHEET TITLE:

PAVING LAYOUT PLAN 1

DATE :	DWG. NO.
09/2023	C-301
SHEET :	
69 OF 190 SHEETS	



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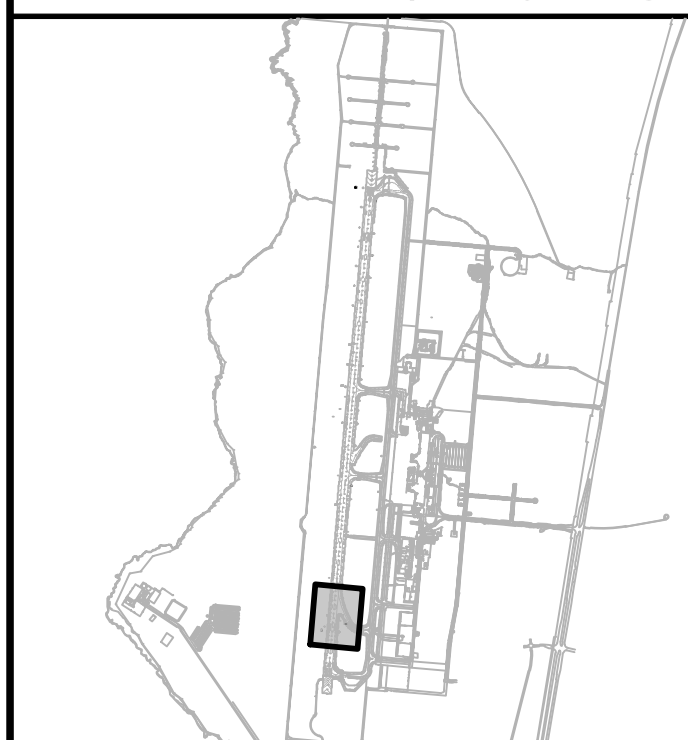


MARTIN A. CLEVEN
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HAWAII, USA
4/30/24
Licensed Expiration Date

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AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
12	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS

SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION

AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:

AH2021-16

SHEET TITLE:

PAVING LAYOUT PLAN 2

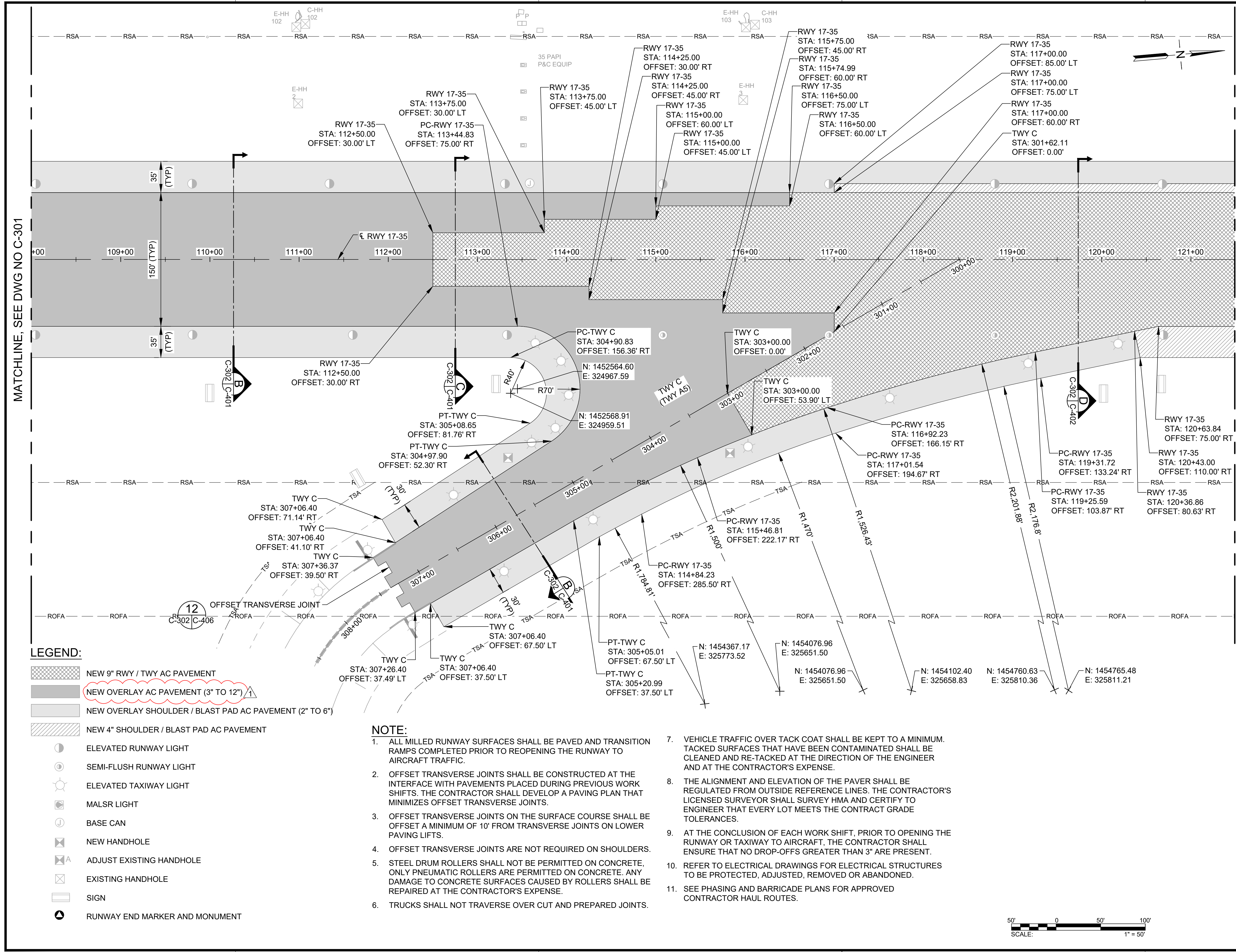
DATE :
09/2023

SHEET :

70 OF 190 SHEETS

DWG. NO.

C-302



MATCHLINE, SEE DWG NO C-301

MATCHLINE, SEE DWG NO C-303

- LEGEND:**
- NEW 9" RWY / TWY AC PAVEMENT
 - NEW OVERLAY AC PAVEMENT (3" TO 12")
 - NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
 - NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
 - ELEVATED RUNWAY LIGHT
 - SEMI-FLUSH RUNWAY LIGHT
 - ELEVATED TAXIWAY LIGHT
 - MALS LIGHT
 - BASE CAN
 - NEW HANDHOLE
 - ADJUST EXISTING HANDHOLE
 - EXISTING HANDHOLE
 - SIGN
 - RUNWAY END MARKER AND MONUMENT

- NOTE:**
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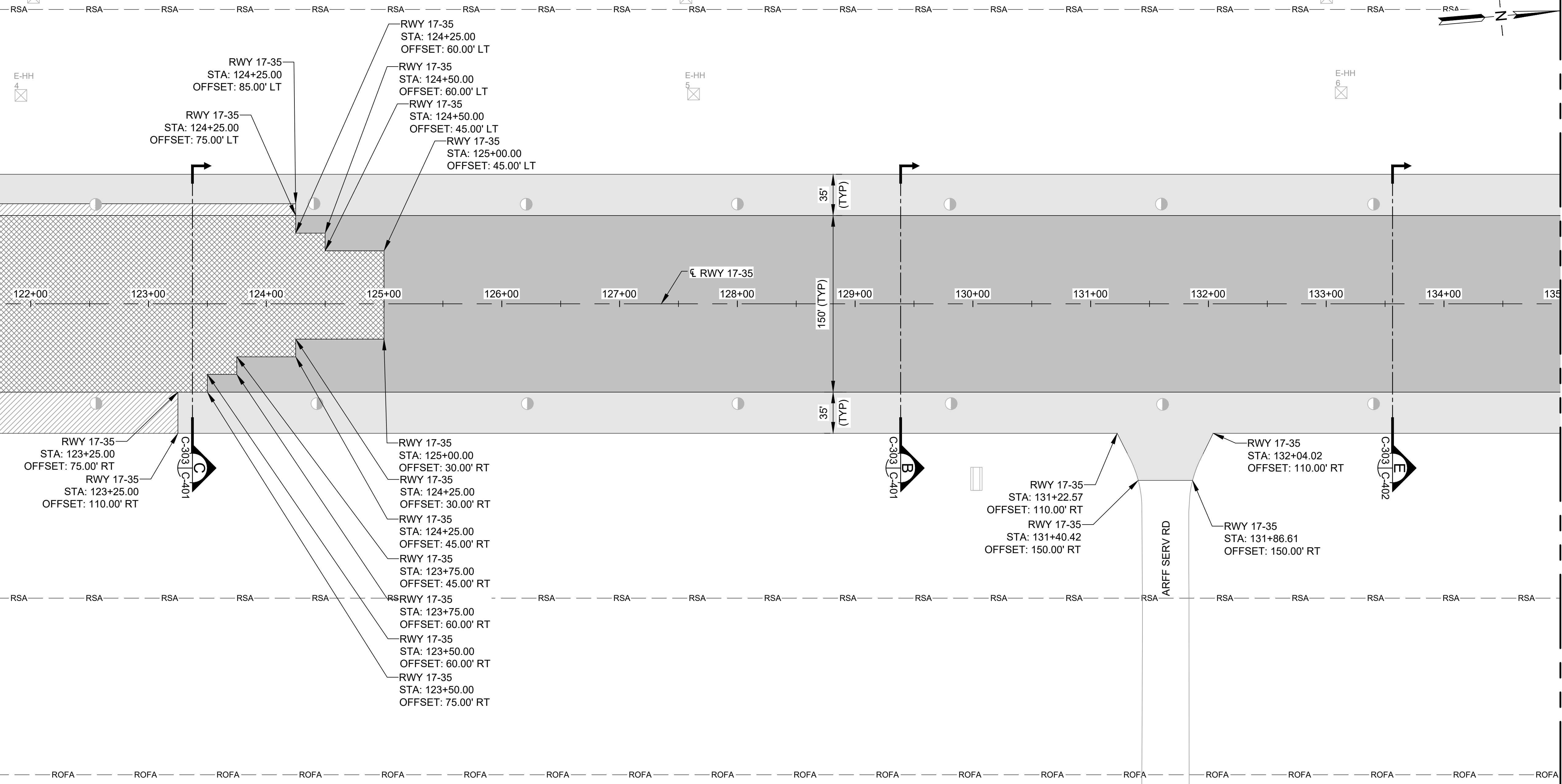


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E-HH 104 C-HH 104

E-HH 105 C-HH 105

E-HH 106 C-HH 106



MATCHLINE, SEE DWG NO C-302

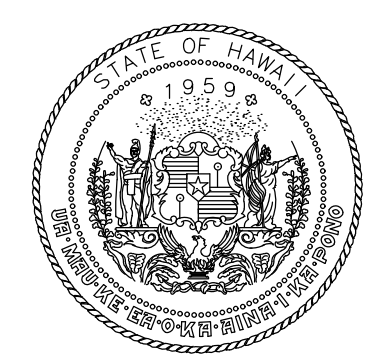
MATCHLINE, SEE DWG NO C-304

LEGEND:

- NEW 9" RWY / TWY AC PAVEMENT
- NEW OVERLAY AC PAVEMENT (3" TO 12")
- NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
- NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
- ELEVATED RUNWAY LIGHT
- SEMI-FLUSH RUNWAY LIGHT
- ELEVATED TAXIWAY LIGHT
- MALS/R LIGHT
- BASE CAN
- NEW HANDHOLE
- ADJUST EXISTING HANDHOLE
- EXISTING HANDHOLE
- SIGN
- RUNWAY END MARKER AND MONUMENT

NOTE:

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11. SEE PHASING AND BARRICADE PLANS FOR APPROVED CONTRACTOR HAUL ROUTES.



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS

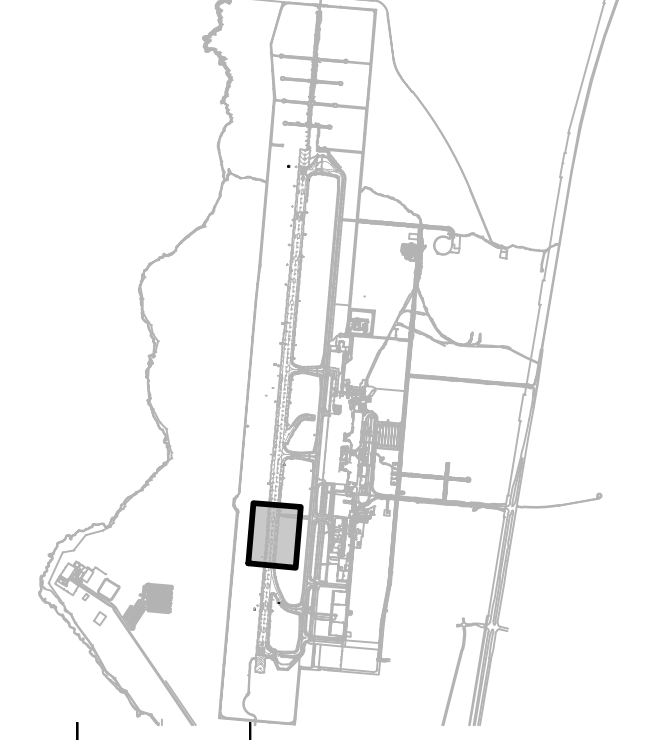


4/30/24
Licensed Expiration Date

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DSGN.	DRWN.	CHKD.	APPD.
AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
1	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

**RUNWAY 17-35
REHABILITATION**

AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:

AH2021-16

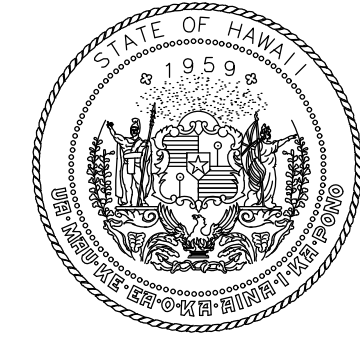
SHEET TITLE:

PAVING LAYOUT PLAN 3

DATE :	DWG. NO.
09/2023	C-303
SHEET :	
71 OF 190 SHEETS	

DWG. NO.
C-303

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STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
AIRPORTS

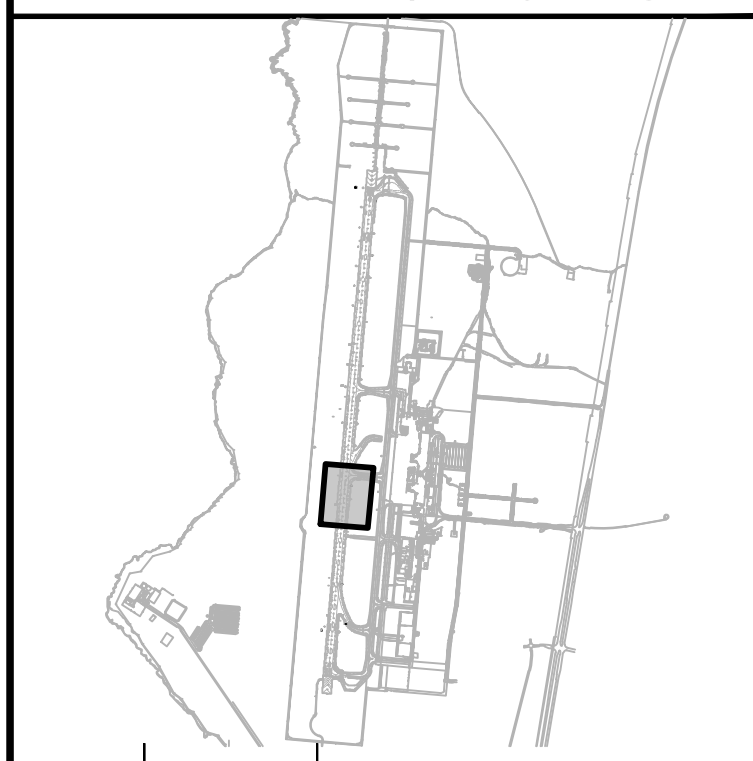


MARTIN A. CLEVEN
LICENSED PROFESSIONAL ENGINEER
No. 12828-C
HAWAII, U.S.A.
4/30/24
Licensed Expiration Date

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DSGN.	DRWN.	CHKD.	APPD.
AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
12	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

**RUNWAY 17-35
REHABILITATION**
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:

AH2021-16

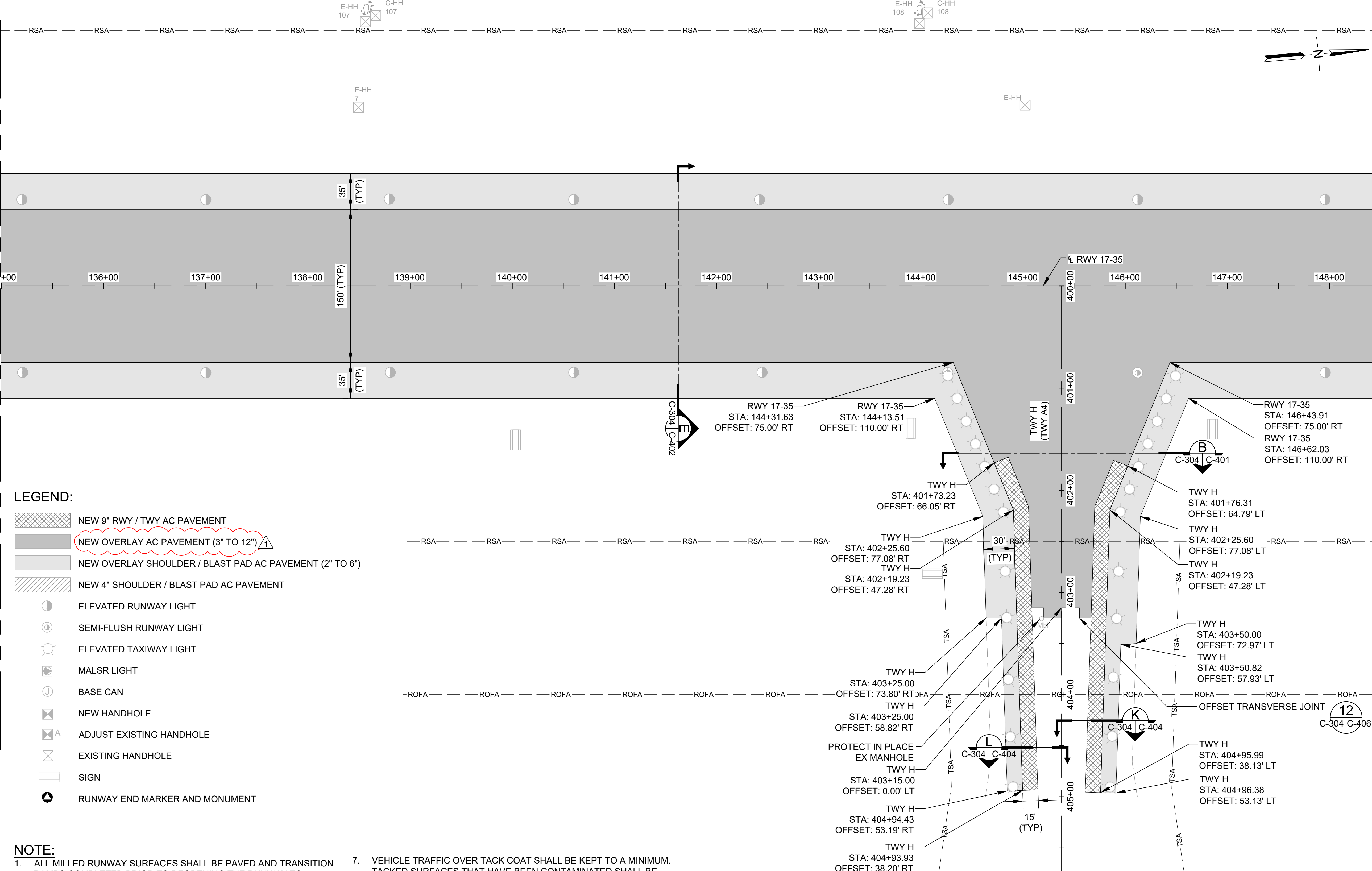
SHEET TITLE:

PAVING LAYOUT PLAN 4

DATE :	09/2023	DWG. NO.	C-304
SHEET :			
72 OF 190 SHEETS			

MATCHLINE, SEE DWG NO C-303

MATCHLINE, SEE DWG NO C-305



LEGEND:

- NEW 9" RWY / TWY AC PAVEMENT
- NEW OVERLAY AC PAVEMENT (3" TO 12")
- NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
- NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
- ELEVATED RUNWAY LIGHT
- SEMI-FLUSH RUNWAY LIGHT
- ELEVATED TAXIWAY LIGHT
- MALSR LIGHT
- BASE CAN
- NEW HANDHOLE
- ADJUST EXISTING HANDHOLE
- EXISTING HANDHOLE
- SIGN
- RUNWAY END MARKER AND MONUMENT

NOTE:

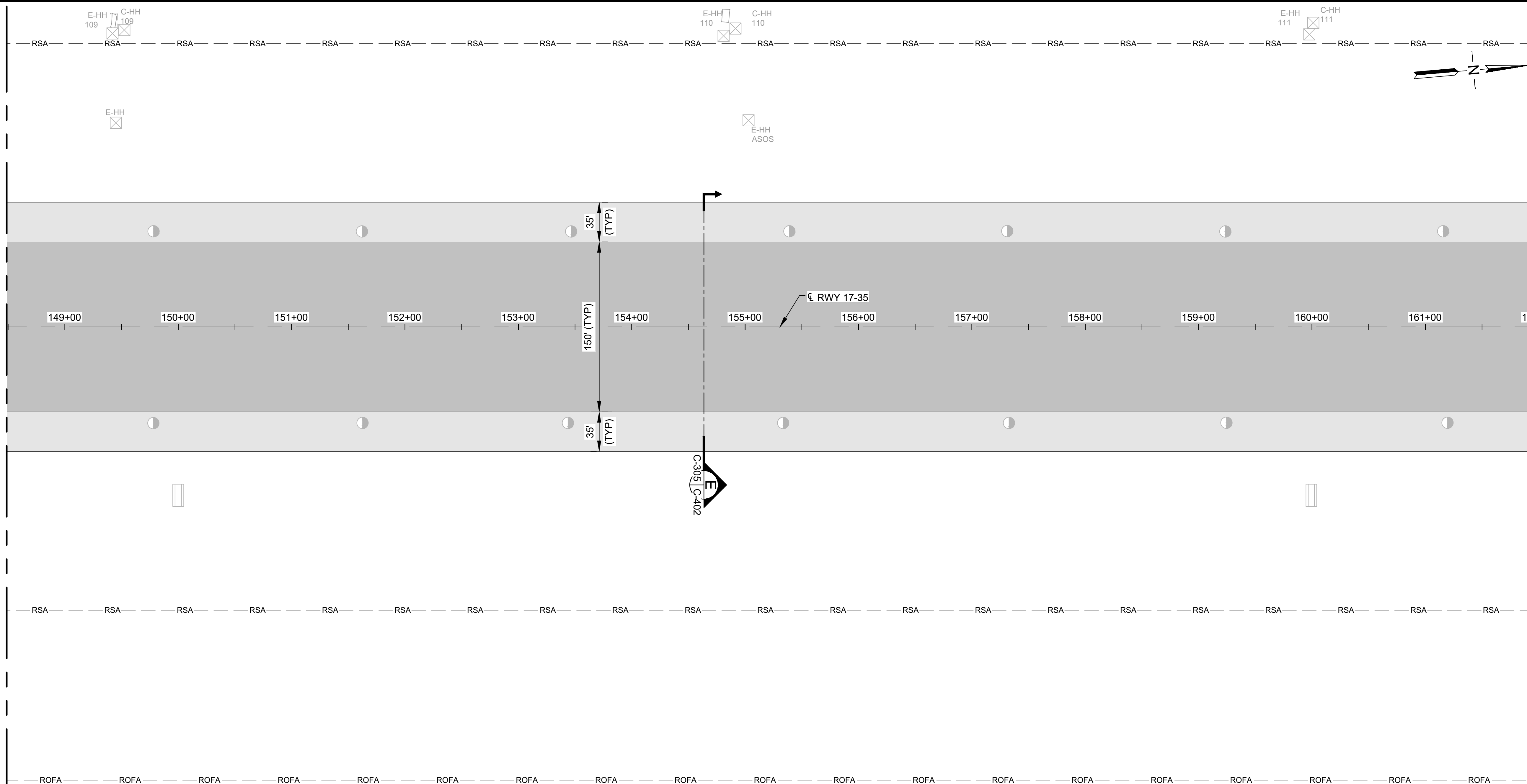
1. ALL MILLED RUNWAY SURFACES SHALL BE PAVED AND TRANSITION RAMPS COMPLETED PRIOR TO REOPENING THE RUNWAY TO AIRCRAFT TRAFFIC.
2. OFFSET TRANSVERSE JOINTS SHALL BE CONSTRUCTED AT THE INTERFACE WITH PAVEMENTS PLACED DURING PREVIOUS WORK SHIFTS. THE CONTRACTOR SHALL DEVELOP A PAVING PLAN THAT MINIMIZES OFFSET TRANSVERSE JOINTS.
3. OFFSET TRANSVERSE JOINTS ON THE SURFACE COURSE SHALL BE OFFSET A MINIMUM OF 10' FROM TRANSVERSE JOINTS ON LOWER PAVING LIFTS.
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11. SEE PHASING AND BARRICADE PLANS FOR APPROVED CONTRACTOR HAUL ROUTES.





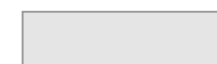











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MATCHLINE, SEE DWG NO C-304

MATCHLINE, SEE DWG NO C-306

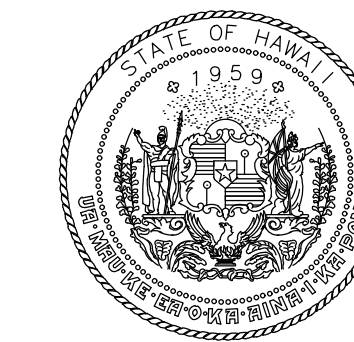
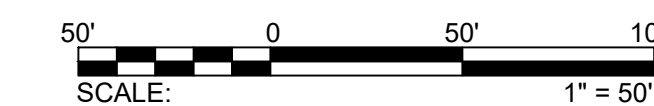


LEGEND:

-  NEW 9" RWY / TWY AC PAVEMENT
-  NEW OVERLAY AC PAVEMENT (3" TO 12")
-  NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
-  NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
-  ELEVATED RUNWAY LIGHT
-  SEMI-FLUSH RUNWAY LIGHT
-  ELEVATED TAXIWAY LIGHT
-  MALS R LIGHT
-  BASE CAN
-  NEW HANDHOLE
-  ADJUST EXISTING HANDHOLE
-  EXISTING HANDHOLE
-  SIGN
-  RUNWAY END MARKER AND MONUMENT

NOTE:

1. ALL MILLED RUNWAY SURFACES SHALL BE PAVED AND TRANSITION RAMPS COMPLETED PRIOR TO REOPENING THE RUNWAY TO AIRCRAFT TRAFFIC.
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STATE OF HAWAII
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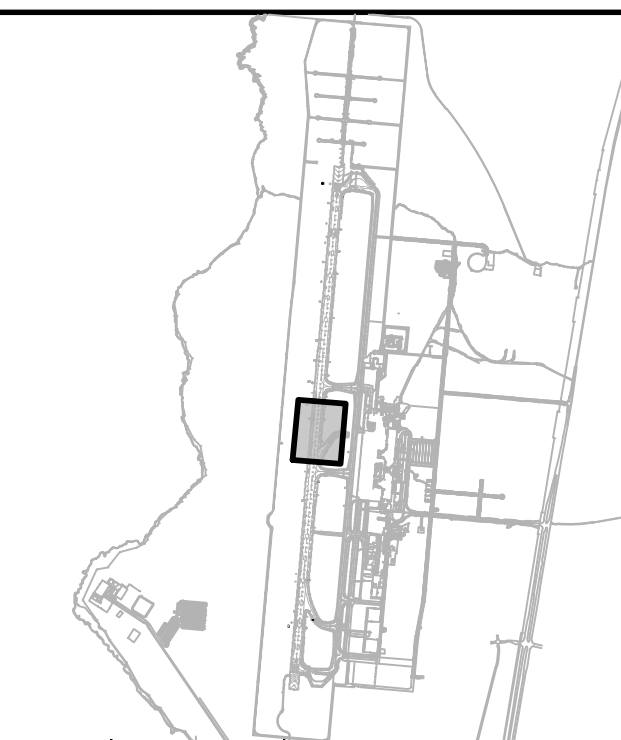


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KEY PLAN / NOTES:



NO.	DATE	REVISIONS
1	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

**RUNWAY 17-35
REHABILITATION**

AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:

AH2021-16

SHEET TITLE:

PAVING LAYOUT PLAN 5

DATE :

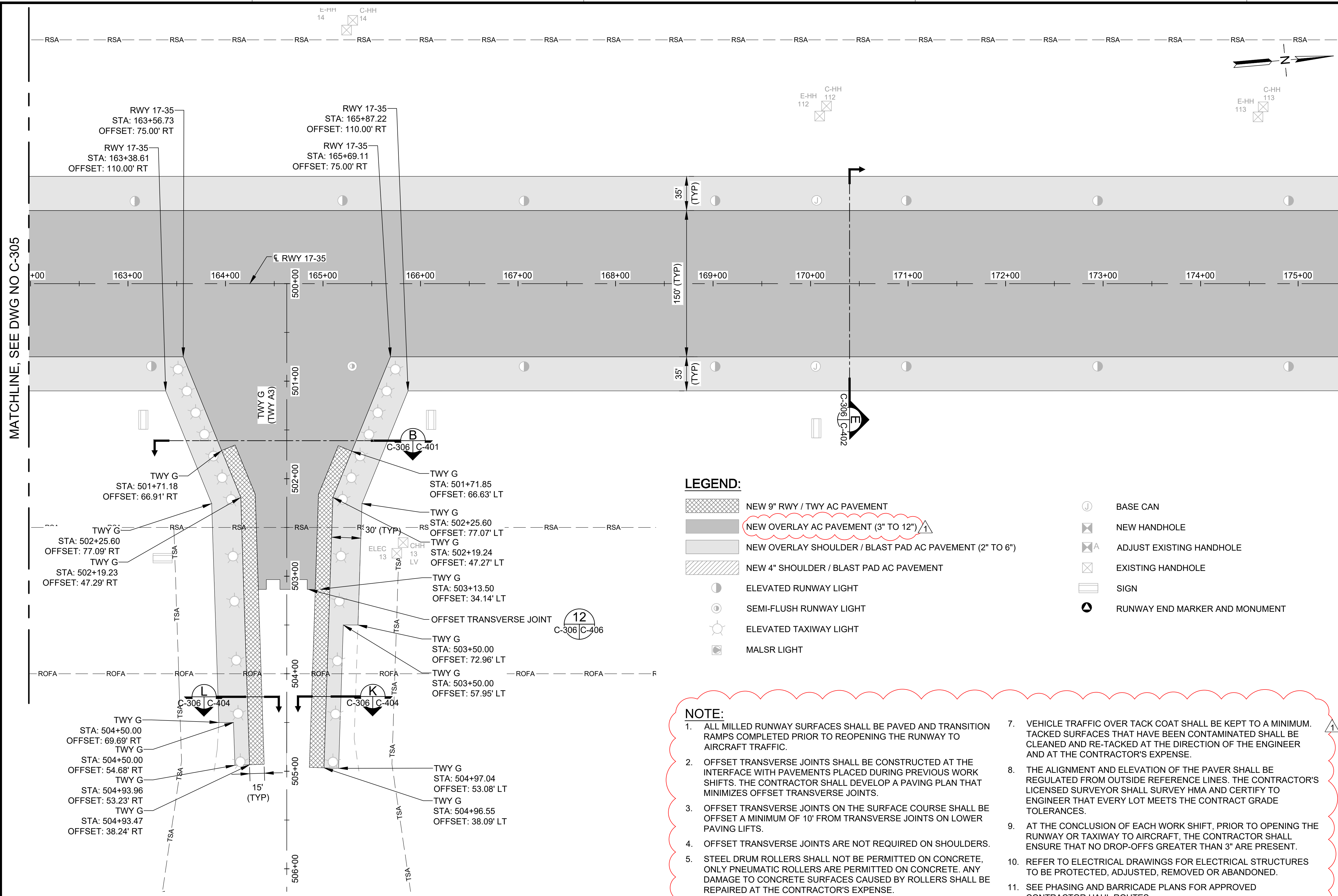
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73 OF 190 SHEETS

DWG. NO.

C-305



MATCHLINE, SEE DWG NO C-305

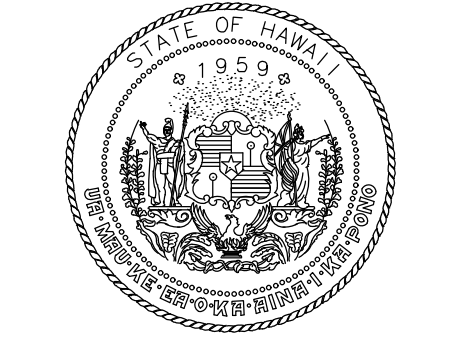
MATCHLINE, SEE DWG NO C-307

LEGEND:

- NEW 9" RWY / TWY AC PAVEMENT
- NEW OVERLAY AC PAVEMENT (3" TO 12")
- NEW OVERLAY SHOULDER / BLAST PAD AC PAVEMENT (2" TO 6")
- NEW 4" SHOULDER / BLAST PAD AC PAVEMENT
- ELEVATED RUNWAY LIGHT
- SEMI-FLUSH RUNWAY LIGHT
- ELEVATED TAXIWAY LIGHT
- MALS LIGHT
- BASE CAN
- NEW HANDHOLE
- ADJUST EXISTING HANDHOLE
- EXISTING HANDHOLE
- SIGN
- RUNWAY END MARKER AND MONUMENT

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CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

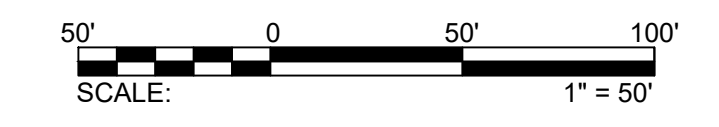
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AH2021-16

SHEET TITLE:

PAVING LAYOUT PLAN 6

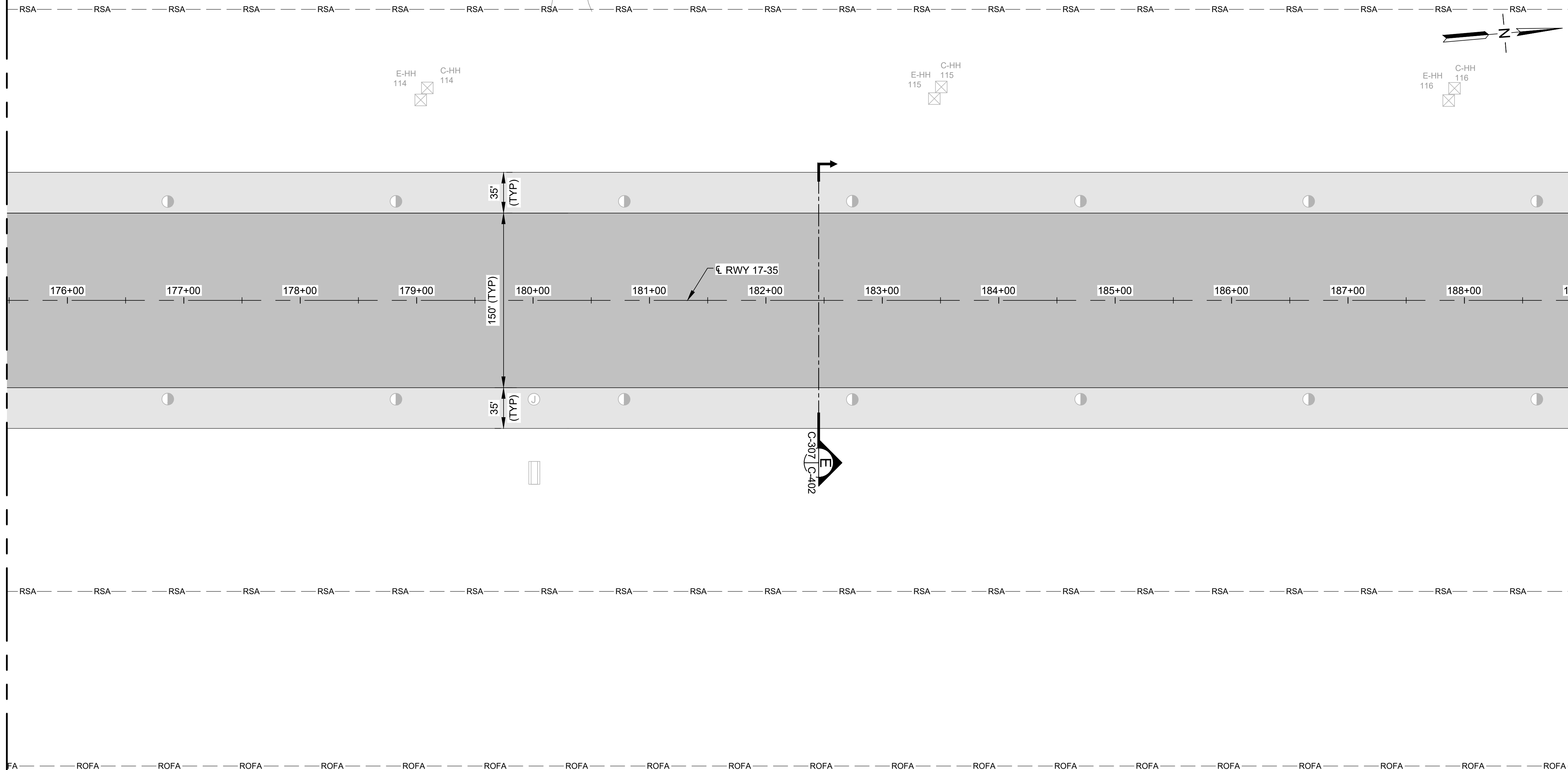
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MATCHLINE, SEE DWG NO C-306

MATCHLINE, SEE DWG NO C-308

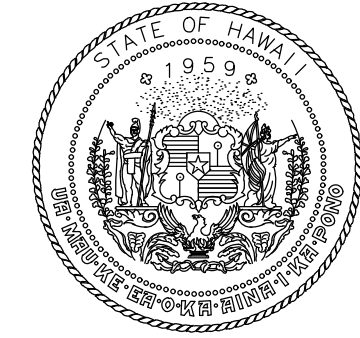
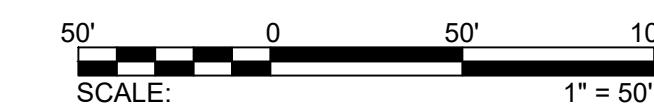


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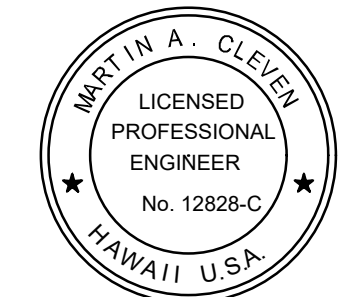
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KEY PLAN / NOTES:



NO.	DATE	REVISIONS
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CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:

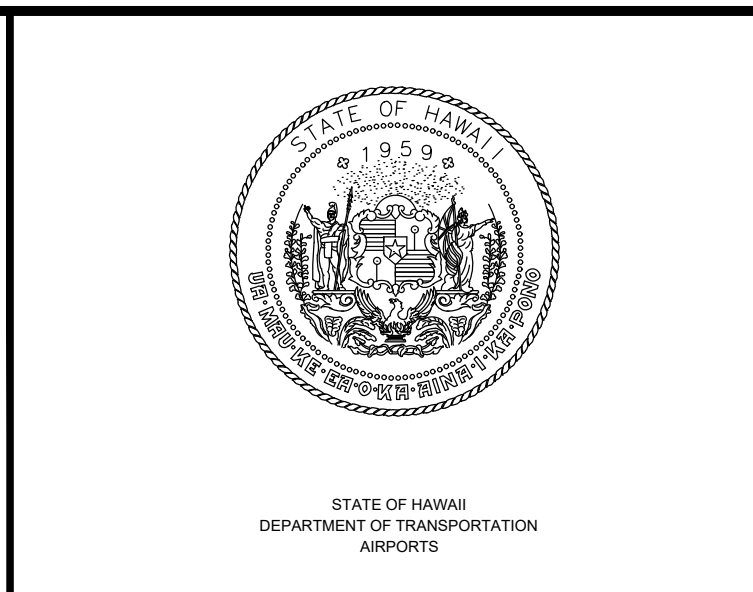
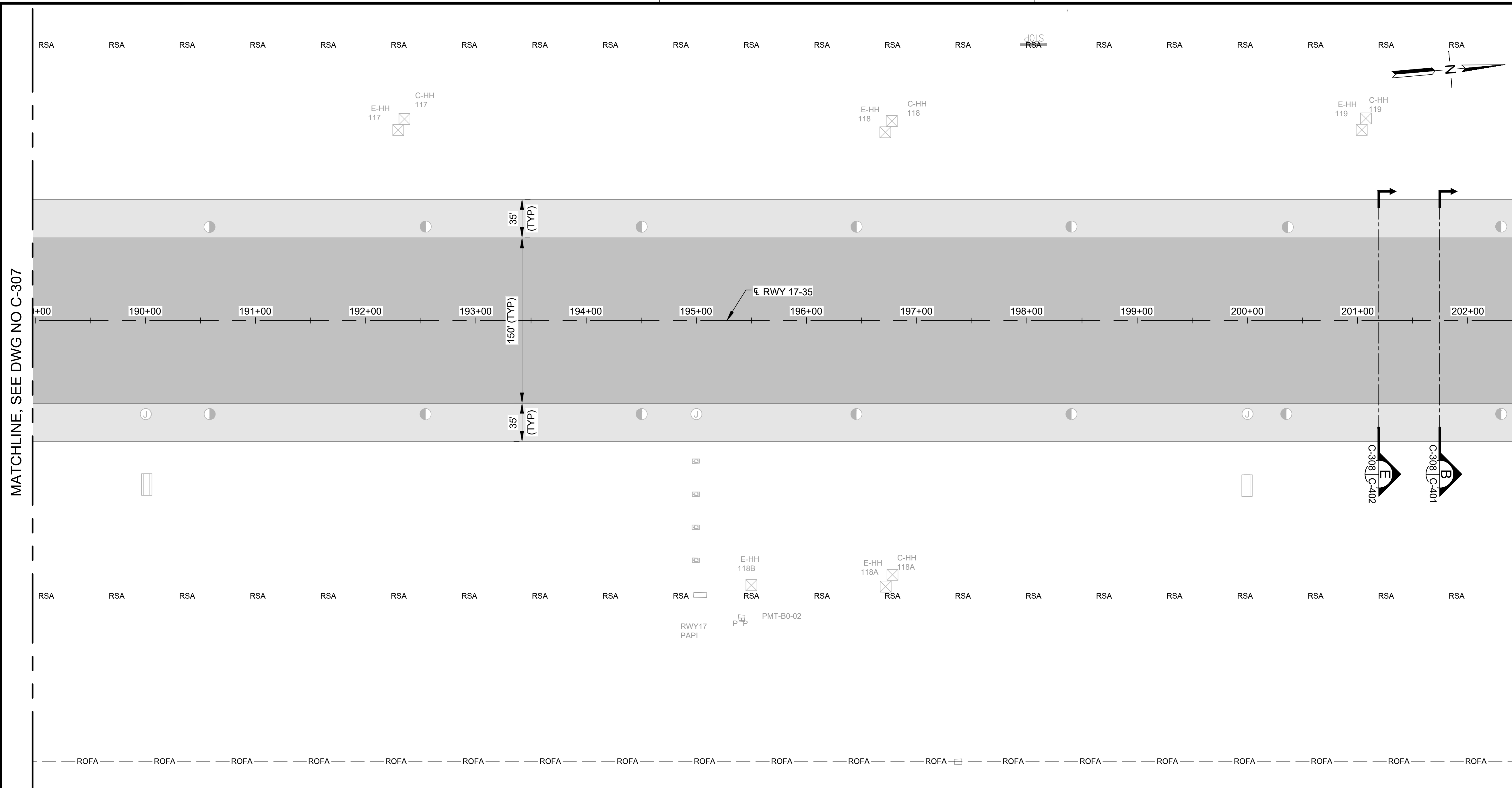
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SHEET TITLE:

PAVING LAYOUT PLAN 7

DATE :	DWG. NO.
09/2023	C-307
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75 OF 190 SHEETS	

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DEPARTMENT OF TRANSPORTATION
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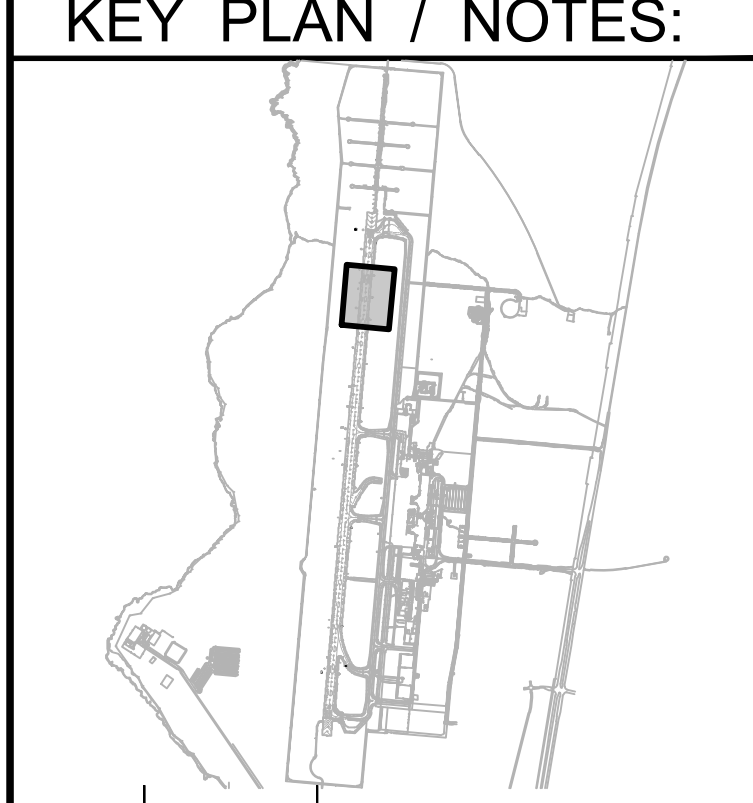
MARTIN A. CLEVELAND
LICENSED PROFESSIONAL ENGINEER
No. 12828-C
HAWAII, U.S.A.

[Signature]
4/30/24
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AC	AT	TR	SH

KEY PLAN / NOTES:



NO.	DATE	REVISIONS
1	12/04/23	ADDENDUM 2

CONSTRUCTION DOCUMENTS
SEPTEMBER 2023
DATE

PROJECT TITLE :
RUNWAY 17-35 REHABILITATION
AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

PROJECT NO.:
AH2021-16

SHEET TITLE:
PAVING LAYOUT PLAN 8

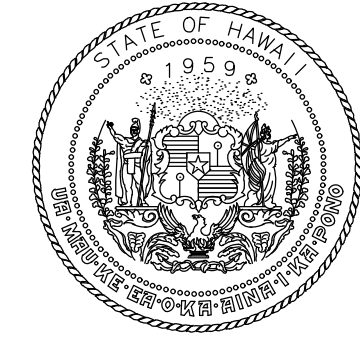
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09/2023	C-308
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76 OF 190 SHEETS	

- LEGEND:**
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 - SIGN
 - RUNWAY END MARKER AND MONUMENT

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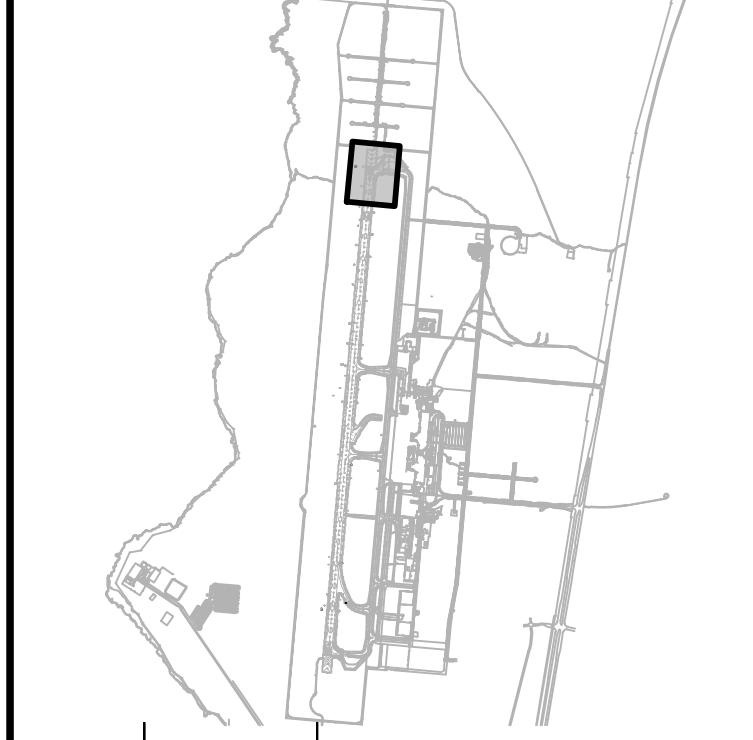


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12/04/23		ADDENDUM 2

CONSTRUCTION DOCUMENTS

SEPTEMBER 2023
DATE

PROJECT TITLE :

RUNWAY 17-35 REHABILITATION

AT
ELLISON ONIZUKA
KONA INTERNATIONAL AIRPORT AT KEAHOLE
KAILUA-KONA, HAWAII

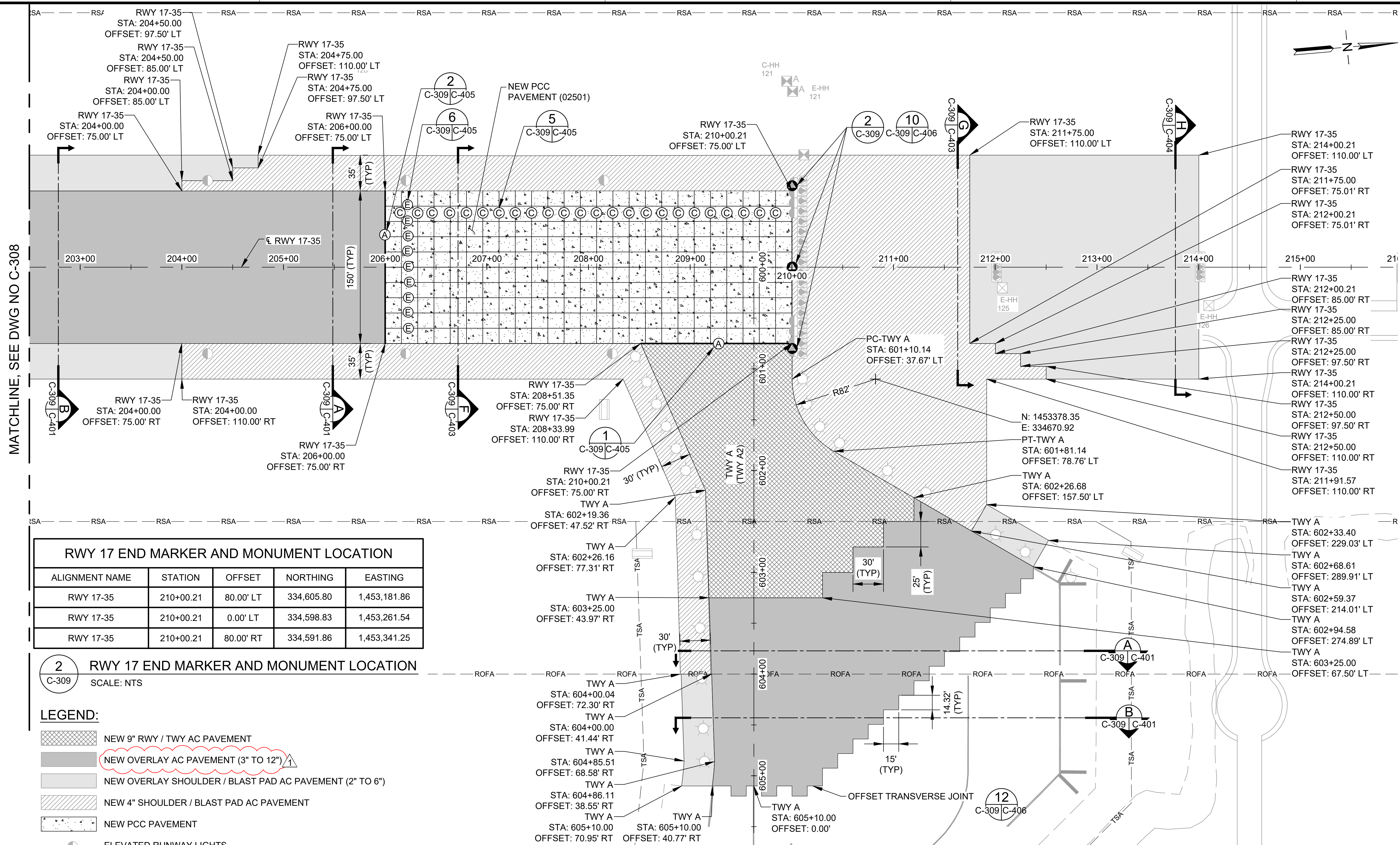
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AH2021-16

SHEET TITLE:

PAVING LAYOUT PLAN 9

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MATCHLINE, SEE DWG NO C-308

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DATE/TIME: November 6, 2023; 2:00 PM HST

LOCATION: State of Hawaii Department of Transportation Airports
Virtual Meeting on Microsoft Teams

PROJECT: Runway 17-35 Rehabilitation
Ellison Onizuka Kona International Airport at Keahole
State Project No. AH2021-16

SUBJECT: Pre-Bid Meeting

MEETING SUMMARY:

I. INTRODUCTIONS

1. Introduction of participants.
2. Attendance list as attached.

II. PROJECT SCOPE

1. Project Overview and Construction Schedule

Project consist of rehabilitation, reconstruction, and repair of Runway 17-35 and connecting taxiways including lighting and signage changes. The rehabilitation will consist of asphalt pavement mill and overlay, new asphalt pavement, and new concrete pavement. Construction is scheduled to last 696 days.

2. Construction Phasing, Access, and Staging

The project will be split into the following phases:

- Phase 1A – Installation of temporary NAVAIDs and airfield lighting changes. Work in this phase will be done during nightly runway closures. The runway must reopen to aircraft each morning.
- Phase 2A – Shorten Runway 17-35. Phase 2A requires marking and lighting changes to shorten the runway. All work must be done during one overnight closure from 2200 on 2/19/2025 to 0600 on 2/20/2025. The runway must reopen to aircraft in the shortened condition on 2/20/2025, and remain for the duration of Phase 2B.
- Phase 2B – Rehabilitate Southern 3,000-ft of Runway. Phase 2B must begin on 2/20/2025 and must be completed on 6/11/2025. Contractor work hours are 1900 to 0700 daily.
- Phase 3A – Shorten Runway 17-35. Phase 3A requires marking and lighting changes to shorten the runway. All work must be done during one overnight closure from 2200 on 6/11/2025 to 0600 on 6/12/2025. The runway must reopen to aircraft in the shortened condition on 6/12/2025, and remain for the duration of Phase 3B.

- Phase 3B – Rehabilitate Northern 3,000-ft of Runway. Phase 3B must begin on 6/12/2025 and must be completed on 10/1/2025. Contractor work hours are 1900 to 0700 daily.
- Phase 4A, 4B, 4C, 4D – Rehabilitate Center Portion of Runway. Contractor work hours are 2200 to 0600 daily. A portion of the runway must remain available for emergency medevac aircraft operations. The runway must reopen to aircraft each morning.
- Phase 5 – Pavement Grooving and Final Marking. Work in this phase will be done during overnight runway closures as shown on the Plans. The runway must reopen to aircraft each morning.

Construction access gates and required security guards for each phase are shown on the Phasing Plans. The contractor’s staging area are also shown on the Plans. All material and equipment must be removed from the work area daily during non-working hours and shall be stored at the staging area indicated on the Plans.

III. GENERAL REQUIREMENTS

1. Bid Phase Schedule

October 26, 2023	Bid Advertisement
November 27, 2023	Last Day of Questions and Substitution Requests
December 11, 2023	Bid Opening at 2:00 PM HST
December 18, 2023	Deadline for Bidders to submit required DBE Forms at 4:30 PM HST. See item 2 below.
2. Required forms. DBE forms provided in the Proposal are required for this project. All bidders must submit these forms within 5-days of bid opening.
3. Unless a change is made by written addendum, nothing stated at this Pre-Bid Meeting shall change the project solicitation. If there is a conflict between what was stated in the pre-bid meeting and the bid documents, the bid documents shall govern.
4. A General Engineering "A" License is required per the Notice to Bidders.
5. This is an FAA AIP funded project. Prospective bidders are reminded to comply with all FAA requirements.
6. Disadvantaged Business Enterprise (DBE) Requirements. Goal 4.5%
7. All work will be done in the Airport Operational Area (AOA). See Airport Division Special Provisions, and Section 01005 and 01565 for AOA security requirements. In addition to the requirements stated in the Contract Bid Documents, all Contractors shall comply with the needs of the Airport Manager.
8. Entry Requirements: AOA badges and vehicle stickers will be required. Proper safety check sticker, insurance and registration will be required.

9. Project duration is 696 Calendar Days from the Notice to Proceed (NTP) date.
10. Liquidated Damages as follows:

FORTY-FIVE THOUSAND DOLLARS (\$45,000.00) per calendar day for failure to begin and end Phases 2B and 3B on the dates noted below.

PHASE	BEGIN DATE	END DATE
2B	2/20/2025	6/11/2025
3B	6/12/2025	10/1/2025

NINE THOUSAND DOLLARS (\$9,000.00) per hour for delay in reopening the runway to aircraft each morning required for Phases 1A, 2A, 3A, 4A, 4B, 4C, 4D, and 5.

FIVE THOUSAND DOLLARS (\$5,000.00) per calendar day for failure to complete the Project within the 696 calendar day duration noted in the Proposal.

11. Allowances - There are restrictions on expenditure of allowances. Expenditures must be justified and change orders must be issued for allowances.
12. Proposal Schedule must be completely filled in even if the proposed cost is \$0 for a line item. Incomplete proposals will be considered irregular.
13. The estimated construction cost is between \$95,000,000 - \$105,000,000.
14. Prior to the bid opening, any changes to the Contract Bid Documents will be issued thru an addendum.
15. The Contractor shall submit all submittals stated in the Specifications after contract award.
16. Should the Contractor find any discrepancies in the Contract Bid Documents during the bidding phase, the Contractor shall promptly notify the State Project Manager. This will mitigate protests and delays to the project.

IV. QUESTIONS

- Each Contractor was given the opportunity to ask questions. Questions are as follows:
 1. Goodfellow Bros.: No questions.
 2. Isemoto Contracting Company: No questions.
 3. Grace Pacific: No questions.
 4. Maui Kupono Builders: No questions.
 5. Road and Highway Builders: No questions.
 6. Nan, Inc: No questions.

7. Jas W. Glover: Are questions to be inputted on HlePRO or emailed to Aren (Orion) and Eddie (HDOT-A)?

Answer (HDOT-A): Questions should be submitted as stated on the Notice to Bidders.

8. Hi'ilawe Construction: No questions.

- Eddie (HDOT-A) clarified that DBE Forms are due to him by 4:30 PM HST on December 18, 2023.
- Site visits for Contractors can be coordinated with Cy Duvauchelle (KOA District).

Meeting adjourned at: 3:30 PM

RESPONSE TO REQUEST FOR INFORMATION (RFI's) / QUESTIONS

AH2021-16 Runway17-35 Rehabilitation at KOA, Kailua-Kona, Hawaii 11/27/2023

1. Please approve the use of DBE forms rev 08.10.22 without the Runway 17-25 project information footer. These forms are very time consuming to set up and it will be much appreciated if the plain version of rev 08.10.22 forms can be used.

HDOT Response: The DBE forms with the Runway 17-35 project information footer shall be used.

2. We request a bid deadline extension of at least 2 more weeks to work on this proposal.

HDOT Response: Bid deadline will be extended by one week. Bids will be due on December 18, 2023.

3. Please provide pavement spot elevation plan showing the existing and finished spot elevations.

HDOT Response: Spot elevations will not be provided. Existing and designed contours are included in the plans. CAD files with existing and designed contours are included in this addendum for informational purposes only.

4. For example, detail B/C-401 calls out "3 inch to 6 inch a.c. pavement surface course" for the runway, but the hatching on Drawing C-301 indicates "new overlay a.c. pavement 3 inch to 9.5 inch" for the same location. Please clarify if 6" a.c. surface course is the maximum thickness required for this section of the runway. Please clarify if the details on Drawing C-401 to C-404 indicates the maximum thickness of a.c. for each runway/taxiway/blast pad section.

HDOT Response: Dwgs C-301 through C-309 differentiate the areas of runway/taxiway reconstruction, shoulder reconstruction, runway/taxiway overlay, and shoulder overlay. Dwgs C-401 through C-404 provide more detailed pavement section information for the approximate station ranges shown on each detail. Dwgs C-301 through C-309 have been revised to show New Overlay AC Pavement (3-in to 12-in). Revised drawings are included in this addendum.

5. Are their as-built drawings of existing electrical signs/fixtures/ductwork/lighting shown on electrical demolition plan sheets E-101 to E-116? If so, please furnish.

HDOT Response: As-built drawings will be provided to the awarded contractor.

6. Are the existing conduits to be removed (shown on demo plan sheets E-101 to E-116 with “R” designation) encased in concrete?

HDOT Response: Contractor shall bid the project assuming that the existing conduits to be demolished are concrete encased.

7. Detail 2 on Sheet E-512 shows existing typical sign foundation to be removed. Detail 2 shows sign concrete foundation to be ~5 ft. thick. Please confirm this dimension.

HDOT Response: Thickness of the sign foundation is unknown, contractor shall bid the removal of sign as shown.

8. Detail 1 on Sheet E-513 shows thickness of new sign foundation pad to be 18” thick. Request concrete pad to be 30” thick uniformly over entire pad (i.e. no “notch” in pad for base can) to expedite completion of sign foundation pads. No changes to reinforcing steel. Pads to be CIP.

HDOT Response: Contractor shall bid per the Contract Documents.

9. Electrical demolition scope of work includes handling and disposal of asbestos piping and materials. This scope of work is unknown at bid time because piping materials and equipment to be removed will need to be tested prior to removal. In order to mitigate costs and ensure all bidders are bidding the same scope, request the handling and disposal of asbestos materials be paid on an allowance basis.

HDOT Response: There are no known asbestos piping or materials on this project. Revised specification Section 16101 (included in this addendum) removes the asbestos piping and materials from the electrical demolition scope of work. No allowance item will be included.

10. Requirements call for builder’s risk insurance covering all risk including tsunamis and earthquake with a limit equal to the full contract price. The engineer’s estimate for this project is \$100,000,000. We recently were asked to provide this same coverage for a \$26,000,000+ project and were unable to find more than \$20,000,000 in coverage for tsunamis and earthquake and the premium for that \$20,000,000 was extremely costly and required multiple insurance companies to build the limits. We also went to over 15 insurance companies to build the \$20,000,000 in limits. We don’t think more than \$20,000,000 will be available in the marketplace so would ask you to reduce the limits required to that amount.

HDOT Response: The Special Provisions (included in this addendum) have been revised to reduce the Builder’s Risk insurance limits to \$20,000,000.

11. Is soil testing required prior to ground disturbance for this project? Or will soil management services be required if suspected contamination is encountered during construction since there is no current testing data indicating whether contamination is/is not encountered?

HDOT Response: A Phase 1 Environmental Site Assessment has been conducted and is included for information in this addendum. Although there is no known soil contamination on the project, any soil disposed off airport property shall be tested per landfill requirements. No soil shall be reused off airport property regardless if test results are below State of Hawaii Department of Health's (DOH) Environmental Action Limits (EALs) and deemed acceptable for reuse. Soil can be reused within the construction site boundaries, as long as testing results indicate that soil is below DOH Unrestricted EALs. All soil removed to landfill must be documented with a soil agreement with the landfill, truck tags, and disposal receipts and summarized in a closure report at the end of the project.

12. For the existing jet blast deflector repainting work, is sand blasting required for surface preparation? Or can hand/mechanical tool surface preparation be used and accepted?

HDOT Response: Per Dwg G-125, Paint Notes 2, preparation prior to painting shall be per ASTM D6386. The contractor shall assume fully weathered galvanization.

13. For the existing jet blast deflector repainting work, does the frame need to be repainted as well or just the corrugated sheet panels?

HDOT Response: Per Dwg G-125, Paint Note 1, only the corrugated sheets are required to be painted.

14. Is there a "Paint Treatment Schedule" for the existing jet blast deflector repainting work?

HDOT Response: A Paint Treatment Schedule will not be provided. Painting shall be as shown on Dwg G-125, Paint Notes 1 through 5.

15. 1. Can we please get a full copy of the geotechnical report?

HDOT Response: The geotechnical data report (included in this addendum) is provided for information.

16. 2. Can we please get the CAD files for the existing and design contours to aid in our pre-bid planning/analysis?

HDOT Response: CAD files with existing and designed contours are included in this addendum for informational purposes only.

17. 3. Will an onsite AC drum plant and/or Concrete Batch Plant be allowed to be setup and operated in the Contractors Staging Area?

HDOT Response: Concrete and asphalt batch plants are permitted under the NOI-C with the following conditions:

- The batch plant must be within the contractor's staging area identified in the plans,
- The batch plant shall not be a commercial operation,
- The plant shall not supply material to other construction projects,
- The plant shall not continue to operate beyond the completion of construction,
- Storm water controls shall be implemented.

In addition, FAA Form 7460 showing the batch plant location and height shall be submitted and approved.

18. 4. Will the contractor be allowed to work on Sundays?

HDOT Response: Contractor's allowable work days are shown on Dwgs G-102 through G-123, Phasing and Barricade Plans. Working Hours includes the days and times when work is allowed in the Air Operations Area (AOA). Contractor's work is allowed at any time in the contractor's staging area.

19. 5. Can the AC pavement debris/RAP/grindings be wasted at the airport in the stockpile they have there?

HDOT Response: AC debris and millings shall be disposed off airport property.

20. 6. Can the concrete debris be crushed and reused as aggregate base, subbase, and/or general fill material?

HDOT Response: Portland cement concrete debris may be crushed and reused as subbase and/or general fill material in accordance with Spec 02152.

21. 7. Please clarify the basis for the quantity shown on the bid form (26,040 CY) for bid item 0252.1? Is this quantity an excavation volume or embankment volume? The bid item description includes both terms and our takeoff quantity for unclassified embankment is close to the 26,040 CY volume listed; our takeoff volume for unclassified excavation (not including any AC pavement or PCCP removal) is much less?

HDOT Response: Bid item 02152.1 includes the embankment volume plus the excavation volume (not including AC or PCC pavement removal).

22. 8. Please clarify how bid item 0252.1 will be measured and paid for?

HDOT Response: Bid item 02152.1 includes the embankment volume plus the excavation volume (not including AC or PCC pavement removal). The contractor shall be paid for the cubic yards of material excavated (measured in the original position), plus for the cubic yards of embankment material (measured in the final position).

23. 9. Please clarify how you calculated the tonnage (i.e., approach, volume conversion assumptions, etc.) for bid items 02401.1 and 02403.1, especially when there are so many variable thicknesses listed on the typical sections? Our takeoff indicates much lower quantities?

HDOT Response: The asphalt pavement quantities are based on an assumed contractor paving plan using variable thickness milling (½-in minimum) required to pave even thickness lifts 15-ft wide. A conversion factor of 2.05 tons/CY was used for all asphalt pavement quantity calculations. The design assumed the top 6-in of new asphalt pavement on runways and taxiways will be surface course (02401), and all other new asphalt pavement will be base/shoulder course (02403). The quantities also included temporary asphalt transition ramps where required, and asphalt pavement for construction of offset transverse joints.

24. 10. Please clarify how you calculated the gallons (i.e., approach, surface areas, rate of application, etc.) for bid items 02602.1 and 02603.1, especially when there are so many variable thicknesses listed on the typical sections? How will these bid items be measured and paid for?

HDOT Response: Prime coat (02602) is only required in the areas designated on the Pavement Sections Dwgs C-401 through C-404. Tack coat (02603) is required on existing or new AC prior to placement of subsequent lifts. Tack coat quantities are based on an assumed contractor paving plan and quantities of new 02401 and 02403 pavements. Items 02602 and 02603 shall be paid at the contract unit price per gallon.

25. 11. To avoid the possibility for further confusion regarding demolition volumes/tonnages, earthwork volumes, aggregate tonnages, and pavement tonnages per phase ---- please provide the complete collection of cross sections used to calculate your quantities? or please provide CAD surfaces for our independant verification and analysis? The project timeline is such that a thorough understanding of the subject quantities per phase is vital in preparing an accurate schedule and price?

HDOT Response: Existing and designed contours are included in the plans. CAD files with existing and designed contours are included in this addendum for informational purposes only.

26. 12. Please consider standardizing the taxiway designations in the plans (specifically in the phasing plans)? EXAMPLE: is it Taxiway H or Taxiway A4 ---- we prefer the designations to be Taxiway A6, A5, A4, A3, and A2 to keep the main taxiway A from being confused with Taxiway A (South) or Taxiway A (North).

HDOT Response: Connector taxiways will be renamed as part of this project. The taxiway designations shown on the phasing plans indicate the taxiway names in use during each phase of work. For example, in Phase 2B, the required NOTAMS indicate that existing taxiways A (south) and C will be closed. When these taxiways reopen during Phase 3B, the renaming will be complete and the new taxiway designations (A6 and A5) will be in use. The remainder of the plans include both the existing and future taxiway names for clarity.

27. Does the concrete foundation for the jet blast deflector need to be painted?

HDOT Response: Per Dwg G-125, Paint Note 1, only the corrugated sheets are required to be painted.

28. Is a cover/containment system that encapsulates the jet blaster deflector needed during surface preparation and painting?

HDOT Response: Contractor shall conduct painting operations consistent with state and federal safety and air quality regulations, and shall meet the requirements of HAR 11-55 Appendix C and the HDOT-A Construction Activities BMP Field Manual. Contractor shall install BMPs for the painting activities to mitigate against airborne paint drift leaving the staging area, and to mitigate against soil or stormwater contamination from paint or solvents as described in BMP Field Manual section C.19. Contractor shall meet the requirements of BMP Field Manual section C.36 for storage, mixing, clean-up, and disposal of paints and solvents.

29. Are QP1 or QP9 a requirement on this project (for painting work)?

HDOT Response: QP1 or QP9 accreditation are not required.

30. Item P-153 CLSM, Section 153-2.1 Materials, a. Cement states: "Cement shall conform to the requirements of ASTM C150 Type I or Type II." Local suppliers on Hawaii Island only use ASTM C-595 Type 1-L Cement, will this cement be deemed suitable?

HDOT Response: Type 1-L cement is acceptable.

31. Item P-501 Cement Concrete Pavement section 501-2.1 Aggregates, b. Fine aggregate states: "Grading of the fine aggregate, as delivered to the mixer, shall conform to the requirements of ASTM C33 and the parameters identified in the fine aggregate material requirements below. Fine aggregate material requirements and deleterious limits are shown in the table below." The local aggregate suppliers on Hawaii Island supply #4 Concrete Sand that does not perfectly meet the ASTM C-33 grading Spec by Note 3 under fine aggregate that states- Fine aggregate that conforms to the grading requirements of specification, prepared by another organization such as a state transportation agency, which is in general use in the area, should be considered as having a satisfactory service record with regard to those concrete properties affected by grading. If the local aggregate supplier produces its Aggregates Per Hawaii Standard Specifications for Road & Bridge Construction, Dated 2005. will that be found acceptable?

HDOT Response: Per ASTM C-33 Grading Note 3, fine aggregate meeting the requirements of the Hawaii Standard Specifications for Road & Bridge Construction, dated 2005, will be acceptable if evidence of satisfactory service record is provided.

32. CONSTRUCTION METHODS section 501-4.5 Handling, measuring, and batching material states: "All Aggregates produced or handled by hydraulic methods, and washed aggregates, shall be stockpile or binned for draining at least 12 hours before being batched." If washed aggregate is specified, the aggregate for this project will be needed to be imported from Oahu or from a mainland source by barge to great expense. No concrete supplier on the Big Island has a wash plant or washes aggregates. We ask to have the requirement for washing aggregate be removed?

HDOT Response: Specification Section 501-4.5 requires washed aggregate to drain prior to batching, and does not require the aggregate to be washed.

33. CONTRACTOR QUALITY CONTROL (CQC) section 501-5.3 CONTRACTOR QC TESTING, a. FINE AGGREGATE, (3) Deleterious substances states: "Fine aggregates as delivered to mixer shall be tested for deleterious substances in fine aggregate for concrete as specified in paragraph 501-2-1b, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances." and under subsection b. COARSE AGGREGATE, (3) Deleterious substances it states: "Coarse aggregates as delivered to mixer shall be tested for deleterious substances in Coarse aggregate for concrete as specified in paragraph 501-2-1b, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances." Please better clarify and/or define the minimum of every 30 days or more frequently during production. Do you mean "every 30 days" of pouring concrete? or "every 30 days" throughout the project? To the best of our knowledge there are no labs in the State of Hawaii that perform this type of testing and the test takes 30 days to run plus shipping time to get the samples to a lab on the mainland. If the stockpiles are inspected and shown to have been crushed from solid basalt rock (naturally having no deleterious substances) and is amply

protected will DOT consider waiving and/or modifying this requirement to help facilitate the necessary schedule constraints on the project? We feel this is a reasonable request.

HDOT Response: Deleterious testing will be required once every 30-days during production of concrete (or more frequently as necessary to control deleterious substances), not every 30-days throughout the project.

34. Item P-610 Concrete for Miscellaneous Structures, 610-2.3 Fine aggregate states: "The fine aggregate for concrete shall meet all fine aggregate requirements of ASTM C33." The local aggregate suppliers on Hawaii Island supply #4 Concrete Sand that does not perfectly meet the ASTM C-33 grading Spec by Note 3 under fine aggregate that states- Fine aggregate that conforms to the grading requirements of specification, prepared by another organization such as a state transportation agency, which is in general use in the area, should be considered as having a satisfactory service record with regard to those concrete properties affected by grading. If the local aggregate supplier produces its Aggregates Per Hawaii Standard Specifications for Road & Bridge Construction, Dated 2005. will that be found acceptable?

HDOT Response: Per ASTM C-33 Grading Note 3, fine aggregate meeting the requirements of the Hawaii Standard Specifications for Road & Bridge Construction, dated 2005, will be acceptable if evidence of satisfactory service record is provided.

35. Item P-610 Concrete for Miscellaneous Structures, 610-2.4 Cement states: "Cement shall conform to the requirements of ASTM C150 Type I or II. The chemical requirements for all cement types specified should meet suitable criteria for deleterious activity. Low alkali cements (less than 0.6% equivalent alkalis)." Local suppliers on Hawaii Island only use ASTM C-595 Type 1-L Cement, will this cement be deemed suitable?

HDOT Response: Type 1-L cement is acceptable.

36. CONSTRUCTION METHODS. 610-3.2 Concrete Mixture states: "The concrete shall develop a compressive strength of 4000 psi in 28 days as determined by test cylinders made in accordance with ASTM C31 and tested in accordance with ASTM C39. The concrete shall contain not less than 470 pounds of cementitious material per cubic yard. The water cementitious ratio shall not exceed 0.45 by weight. The air content of the concrete shall be 5% +/- 1.2% as determined by ASTM C231 and shall have a slump of not more than 4 inches as determined by ASTM C143." The State of Hawaii is not in a freeze thaw weathering region per ACI 301 Exposure Class F. Hawaii falls under class F, F0 and to the best of our knowledge (at the Airport) is not exposed to cycles of freezing and thawing. Per ACI Table 4.2.2.7.b Exposure Class F0 Air Content please consider removing this requirement?

HDOT Response: This project is FAA funded. Air content for structural concrete shall be 5% +/- 1.2% on FAA projects.

37. 14. Please provide clarification on how the contractor should quantify the lengths and widths of existing cracks in the runway that need repair? Plan sheet C-405 detail 3 depicts such crack repair, yet there are no plans specifically depicting the extent to which such a detail should be applied?

HDOT Response: The lengths and widths of cracks in the milled surface is unknown. After milling and cleaning operations, the RPR will identify the cracks greater than ½-in wide that require repair.

38. 15. Please provide as-built information pertaining to the existing lighting conduit/wiring/fixtures that will require possible replacement when the shoulder grading/paving is performed along the runway? Can a temporary lighting system be installed just outside the disturbance zone prior to any grading activities to ensure work and system continuity through-out the work window?

HDOT Response: As-built drawings will be provided to the awarded contractor. Temporary edge lighting can be used if necessary.

39. 16. Is there a water source readily available at the Airport for the contractor's use during construction? if so, will the water be provided at the source free of charge to the contractor?, if not, what is the cost of the water, what is the unit of measure for such charges, and how much water is available on a per day basis?

HDOT Response: Water is available for use on this project from nearby fire hydrants. A temporary water meter shall be installed on the hydrant and the contractor shall be responsible for paying Department of Water Supply rates for the water usage. Sufficient water supply is anticipated, however, limits on water shall be as directed by the Department of Water Supply.

40. 17. Will the contractor be allowed to work on Saturdays and Sundays? Please better clarify Phase 1B, plan sheet G-104 indicates a duration of 2 calendar days, yet the schedule on plan sheet G-101 indicates 45 calendar days?

HDOT Response: Contractor's allowable work days are shown on Dwgs G-102 through G-123, Phasing and Barricade Plans. Working Hours includes the days and times when work is allowed in the Air Operations Area (AOA). Contractor's work is allowed at any time in the contractor's staging area. Phase 1B is the waiting period for FAA flight inspection. The total duration of the waiting period for FAA to schedule the flight check is 45 days. The actual flight inspection is anticipated to be 2 days from 0600-0900, during which the contractor shall close the runway and provide temporary markings per G-102.

41. 18. Also, Plan Sheet G-123 indicates 78 calendar days but plan sheet G-101 indicates 71 calendar days? Also, please clarify the two (2) work periods indicated 0000-0600 (HST), 1000-1600 (UTC) --- assuming this is the same period of 6 hours, with two (2) time zone standards referenced?

HDOT Response: The duration of Phase 5 shall be 78 days. Updated Dwg G-101 is included in this addendum.

42. 19. Please provide all the necessary steps required to arrange a site visit and/or let us know when the site visit is scheduled for the next group?

HDOT Response: As discussed in the Pre-Bid meeting, contractors may coordinate site visits by contacting Cy Duvauchelle, KOA Airport Superintendent by email at cy.duvauchelle@hawaii.gov or by phone at 808-938-9198.

43. Item P153 CLSM 153-2.1 Materials. a. Cement. Cement shall conform to the requirements of ASTM C150 Type I or Type II. Supplier of Cement will be switching from ASTM C-150 Type I /II cement to ASTM C-595 Type 1-L Cement in December 2023. Please confirm acceptance of this material.

HDOT Response: Type 1-L cement is acceptable.

44. Item P-501 Cement Concrete Pavement 501-2.1 Aggregates. b. Fine aggregate. Grading of the fine aggregate, as delivered to the mixer, shall conform to the requirements of ASTM C33 and the parameters identified in the fine aggregate material requirements below. Fine aggregate material requirements and deleterious limits are shown in the table below. Our local aggregate supplier # 4 Concrete Sand does meets ASTM C-33 grading Spec by Note 3 under fine aggregate that states- Fine aggregate that conforms to the grading requirements of specification, prepared by another organization such as a state transportation agency, which is general use in the area, should be considered as having a satisfactory service record with regard to those concrete properties affected by grading. West Hawaii Concrete produce all Aggregate Per Hawaii Standard Specifications for Road & Bridge Construction, Dated 2005. Please confirm acceptance.

HDOT Response: Per ASTM C-33 Grading Note 3, fine aggregate meeting the requirements of the Hawaii Standard Specifications for Road & Bridge Construction, dated 2005, will be acceptable if evidence of satisfactory service record is provided.

45. CONSTRUCTION METHODS 501-4.5 Handling, measuring, and batching material. All Aggregates produced or handled by hydraulic methods, and washed aggregates, shall be stockpile or binned for draining at least 12 hours before being batched. If washed aggregate is specified, the aggregate for this project will be needed to be imported from outer island or mainland. No concrete supplier on the Big Island has a wash plant. We ask to have the requirement for washing aggregate be removed. Please confirm if the aggregate will need to follow spec and be imported.

HDOT Response: Specification Section 501-4.5 requires washed aggregate to drain prior to batching, and does not require the aggregate to be washed.

46. 501-5.3 CONTRACTOR QC TESTING. a. FINE AGGREGATE (3) Deleterious substances. Fine aggregates as delivered to mixer shall be tested for deleterious substances in fine aggregate for concrete as specified in paragraph 501-2-1b, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances. b. COARSE AGGREGATE (3) Deleterious substances. Coarse aggregates as delivered to mixer shall be tested for deleterious substances in Coarse aggregate for concrete as specified in paragraph 501-2-1b, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances. Can you define the minimum of every 30 days or more frequently during production. If this every 30 days of pouring concrete or ever 30 days thru out the project? There is no lab in the state of Hawaii that performs this testing requirement, the test take's 30 days to run plus shipping time to get the samples to the lab on the mainland.

HDOT Response: Deleterious testing will be required once every 30-days during production of concrete (or more frequently as necessary to control deleterious substances), not every 30-days throughout the project.

47. Item P-610 Concrete for Miscellaneous Structures 610-2.3 Fine aggregate. The fine aggregate for concrete shall meet all fine aggregate requirements of ASTM C33. Our local aggregate supplier # 4 Concrete Sand does meets ASTM C-33 grading Spec by Note 3 under fine aggregate that states- Fine aggregate that conforms to the grading requirements of specification, prepared by another organization such as a state transportation agency, which is general use in the area, should be considered as having a satisfactory service record with regard to those concrete properties affected by grading. West Hawaii Concrete produce all Aggregate Per Hawaii Standard Specifications for Road & Bridge Construction, Dated 2005. Please confirm acceptance.

HDOT Response: Per ASTM C-33 Grading Note 3, fine aggregate meeting the requirements of the Hawaii Standard Specifications for Road & Bridge Construction, dated 2005, will be acceptable if evidence of satisfactory service record is provided.

48. **610-2.4 Cement.** Cement shall conform to the requirements of ASTM C150 Type I or II. The chemical requirements for all cement types specified should meet suitable criteria for deleterious activity. Low alkali cements (less than 0.6% equivalent alkalies). Supplier of Cement will be switching from ASTM C-150 Type I/II cement to ASTM C-595 Type 1-L Cement in December 2023. Please confirm acceptance.

HDOT Response: Type 1-L cement is acceptable.

49. **CONSTRUCTION METHODS 610-3.2 Concrete Mixture.** The concrete shall develop a compressive strength of 4000 psi in 28 days as determined by test cylinders made in accordance with ASTM C31 and tested in accordance with ASTM C39. The concrete shall contain not less than 470 pounds of cementitious material per cubic yard. The water cementitious ratio shall not exceed 0.45 by weight. The air content of the concrete shall be 5% +/- 1.2% as determined by ASTM C231 and shall have a slump of not more than 4 inches as determined by ASTM C143. The State of Hawaii is not in a freeze thaw weathering region per ACI 301 Exposure Class F. Hawaii falls under class F, F0 Not exposed to cycles of freezing and thawing. Per ACI Table 4.2.2.7.b Exposure Class F0 Air Content N/A.

HDOT Response: This project is FAA funded. Air content for structural concrete shall be 5% +/- 1.2% on FAA projects.

50. **Note 2 on Dwg. No. C-306 states, "ON UPPER PAVING LIFTS, THE CONTRACTOR SHALL PROVIDE ADEQUATE LABOR, MATERIAL, AND EQUIPMENT NECESSARY TO CONSTRUCT A MINIMUM OF 600' OF RUNWAY LENGTH (FULL WIDTH) PLUS OFFSET TRANSVERSE JOINTS AND TRANSITION RAMPS DURING EACH OVERNIGHT CLOSURE."** Please confirm that this production requirement is noted correctly. If the requirement is noted correctly, please confirm the extent of this requirement, as it is only appears on 1 of the 9 "Paving Layout Plan" drawings.

HDOT Response: Notes 1 and 2 on Dwg C-306 shall be disregarded. Revised drawing are included in this addendum.

51. **Given the variability shown in the typical sections, can cross sections showing the required/designed structural section be provided on 50' or 100' intervals?**

HDOT Response: Existing and designed contours are included in the plans. CAD files with existing and designed contours are included in this addendum for informational purposes only.

52. Given the required production rates to meet the schedule, will an asphalt plant be required on site? Can a location be designated for this use?

HDOT Response: An on-site asphalt plant is not required. However, on-site plants are permitted under the NOI-C with the following conditions:

- The batch plant must be within the contractor's staging area identified in the plans,
- The batch plant shall not be a commercial operation,
- The plant shall not supply material to other construction projects,
- The plant shall not continue to operate beyond completion of construction,
- Storm water controls shall be implemented.

In addition, FAA Form 7460 showing the batch plant location and height shall be submitted and approved.

53. Note 4 on the "Civil Demolition Plan" drawings (Dwg. No. C-201 - C-209) states that "Contractor shall perform milling and overlay across the entire runway width during each overnight closure". Please confirm this note only applies to work in Phase 4. Also, does "the entire runway width" include the runway shoulders?

HDOT Response: The note applies to Phase 4, and any other work that requires overnight runway closure and reopening the following day. The "entire runway width" does not include the shoulders, however, no vertical drops greater than 3-in are allowed in the runway and taxiway shoulders or safety areas when the runway or taxiway are open to aircraft operations.

54. Note 6 on the "Civil Demolition Plan" drawings (Dwg. No C-201 - C-209) states, "FOLLOWING MILLING OPERATIONS, THE RPR SHALL INSPECT THE MILLED SURFACE TO DETERMINE IF THE CONDITION OF THE UNDERLYING PAVEMENT IS SATISFACTORY. THE CONTRACTOR SHALL MILL ADDITIONAL DEPTH AS DIRECTED BY THE RPR." Please confirm the method of measurement and payment that will be used for this additional milling work.

HDOT Response: Additional milling, as directed by the RPR, beyond what is shown on the contractor's approved paving plan shall be paid at the contract unit price per square yard for milling.

55. Given the ongoing volatility in asphalt binder pricing/local supply and that this project will not begin paving until 2025, respectfully request that a price adjustment contract provision be added to avoid asphalt binder price speculation that will likely lead to inflated bid pricing to protect against possible binder price increases.

HDOT Response: A price adjustment contract provision will not be added.

56. It was stated in the pre-bid meeting that dates for Phase 2A and Phase 3A are hard dates that cannot be adjusted. If all work is finished in Phase 2B in less than the 111 days scheduled, can the contractor start work in Phase 3A prior to 6/11/25 or would this date still need to be held?

HDOT Response: Dates for work in Phase 2A, 2B, 3A, and 3B are hard dates that cannot be adjusted. Work cannot begin for these phases until the dates shown on the Phasing Plans.

57. Will asphalt material used for temporary transition ramps be measured and paid? Also, will cold milling to remove the temporary transition ramps be measured and paid?

HDOT Response: Asphalt material for transition ramp construction shall be paid at the contract unit price per ton. Milling to remove the transition ramp will be paid at the contract unit price per square yard.

58. Section 02152 states "Rock, cement concrete pavement, asphalt pavement, and other embankment material shall not be disposed of except at places and in the manner designated on the plans or by the RPR." Please confirm the disposal location for these materials.

HDOT Response: Rock, cement concrete pavement, and other suitable materials shall be reused at other locations on the project, provided the materials meet the gradation requirements in 02152. Asphalt cement pavement debris and millings shall be disposed off airport property.

59. Note 5 on the "Civil Demolition Plan" drawings (Dwg. No. C-201 - C-209) states, "NO DROP-OFFS ARE PERMITTED ON RUNWAY OR TAXIWAY PAVEMENT WHEN OPEN TO AIRCRAFT." Note 7 states, "AT THE CONCLUSION OF EACH WORK SHIFT, PRIOR TO REOPENING THE RUNWAY OR TAXIWAY TO AIRCRAFT TRAFFIC, THE CONTRACTOR SHALL ENSURE THAT NO DROP-OFFS GREATER THAN 3" ARE PRESENT WITHIN THE RUNWAY OR TAXIWAY SAFETY AREA." Are drop-offs less than 3" allowed on runway or taxiway shoulder pavement when open to aircraft?

HDOT Response: Both notes are correct. No drop-offs are allowed in the runway or taxiway pavement. However, drop-offs less than 3-in are allowed in the shoulders and safety areas (outside of the runway and taxiway pavement).